

# LEGACY CAPITOL HILL

1525 11TH AVE

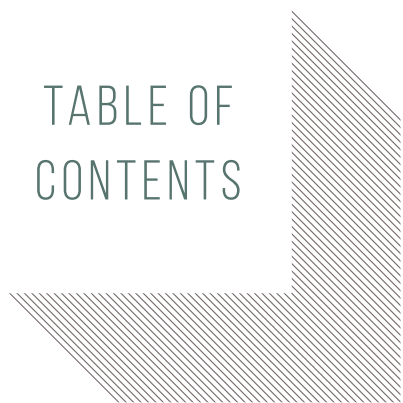
RECOMMENDATION MEETING  
1/25/2017  
MUP #3023226



PLACE







# TABLE OF CONTENTS

## PROPOSAL

Project Information .....	3
---------------------------	---

## CONTEXT ANALYSIS

Surrounding Uses and Transportation .....	4
Vicinity Axonometric.....	6

## ZONING DATA

Zoning Summary .....	6
----------------------	---

## SITE

Composite Site Plan.....	7
--------------------------	---

## ITEMIZED RESPONSE TO EDG

Preservation Plan (Presented at EDG).....	8
Approved EDG Massing Option.....	9
EDG Guidance Overview .....	10
Theme 1: Massing and Design .....	12
Theme 2: Relationship to Neighboring Structures .....	16
Theme 3: Streetscape and Ground Floor .....	20
Theme 4: Service Uses .....	26

## FLOOR PLANS

Floor Plans .....	30
-------------------	----

## LANDSCAPE

Landscape Plan.....	34
---------------------	----

## ELEVATIONS AND MATERIALS

Material and Color Palette.....	36
Elevations .....	38

## LIGHTING

Exterior Lighting Plan .....	42
------------------------------	----

## BUILDING SECTIONS

Building Sections.....	44
------------------------	----

## RENDERINGS

Renderings .....	46
------------------	----

## DEPARTURES

Departure Summary .....	56
Departures.....	57

## OTHER

Shadow Studies .....	62
Sightline Studies.....	64



PROJECT INFORMATION

ADDRESS: 1525 11TH AVE		
DPD PROJECT #: 3023226		
ZONE: NC3P-65, PIKE/PINE CONSERVATION OVERLAY		
ARCHITECT:	LANDSCAPE ARCH:	DEVELOPER:
ANKROM MOISAN	PLACE	LEGACY COMMERCIAL
1505 5th Ave., Ste. 300	107 Spring St.	400 112 Ave. NE, Ste. 230
Seattle, WA 98101	Seattle, WA 98104	Bellevue, WA 98004
206.576.1600	503.334.2080	425.460.4374
CONTACT:	CONTACT:	CONTACT:
Phillip Bozarth-Dreher	Phoebe Bogert	Monica Aydelotte

DEVELOPMENT OBJECTIVES

The proposed project is an office building addition to a listed Seattle Landmark, including below grade parking. The basic program is as follows:

- 65,000 sf of office area
- 12,000 sf of ground floor retail
- 5 Stories of total height, plus a 1 story basement
- 34 parking stalls

PROJECT GOALS

Preserve the character defining aspects of the landmark Kelly-Springfield Building

- Restore the primary 11th Avenue facade.
- Preserve elements of the interior and side walls in order to convey the character of the original building as a whole.

Strengthen the character of the Pike/Pine neighborhood

- Incorporate materials and architectural elements that strengthen the area’s auto-row vernacular.
- Enhance the block’s sidewalk experience.

Bolster the daytime vitality of the area

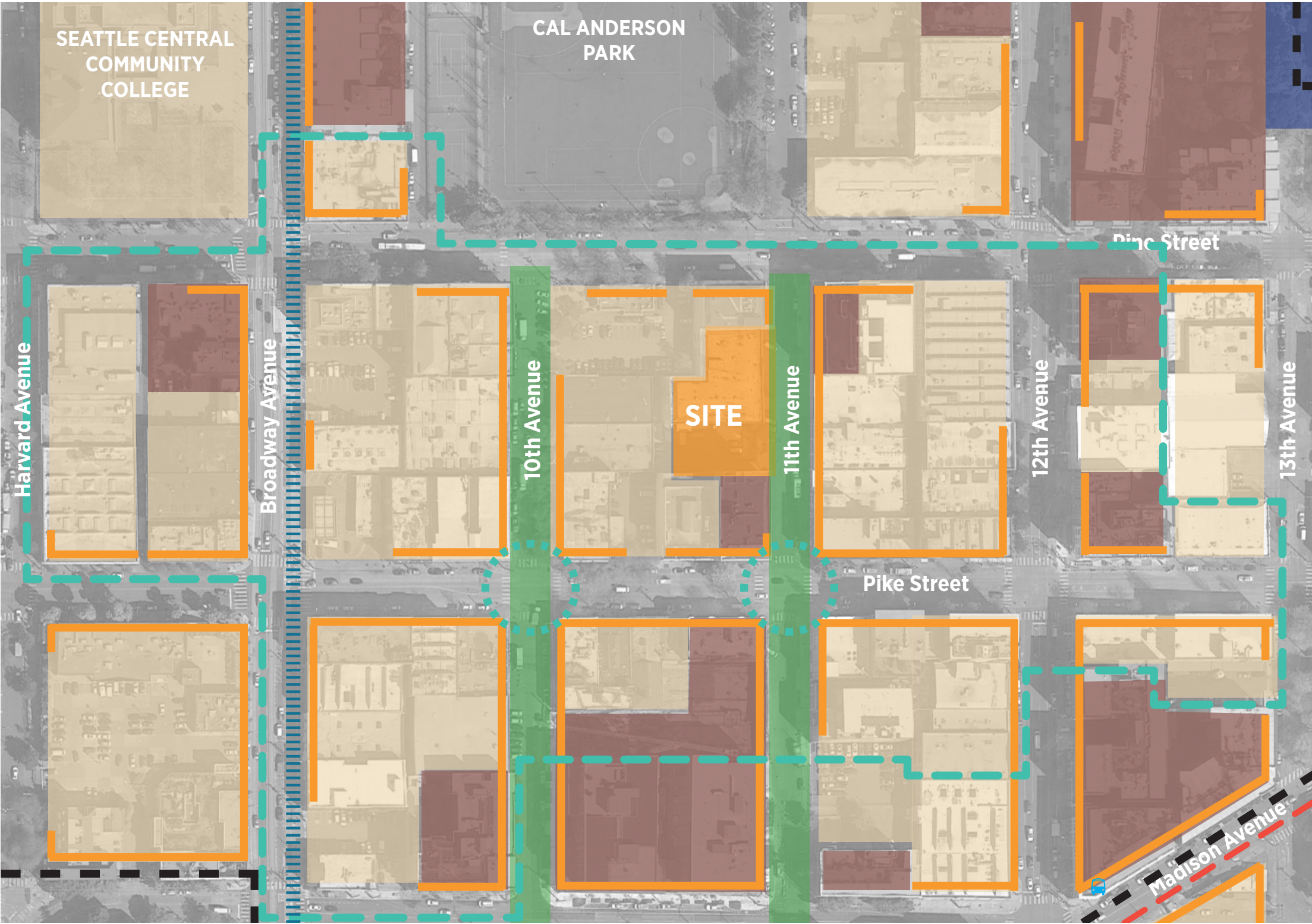
- Introduce a mix of daytime uses that support existing neighborhood businesses.
- Create retail and office spaces that are adaptable to various tenant types, including businesses already in the area.





SURROUNDING USES AND TRANSPORTATION

- Primarily Residential Use
- Primarily Commercial or Institutional Use
- Tenth and Eleventh Avenue Street Design Concept Plan
- Streetcar Line
- Principal Arterials
- Ground Floor Retail
- Prominent Intersection
- Outline of the Pike Pine Conservation Overlay District
- Outline of the Pike Pine Conservation Core
- LR3 Zone





## VICINITY AXONOMETRIC

### GROUND FLOOR USES

Most properties south of Pine Street and surrounding the site feature continuous or near continuous ground floor retail spaces, largely occupied by restaurant or nightlife uses.

### UPPER LEVEL USES

New buildings in the area have been almost exclusively residential on the upper levels, but many of the older buildings feature office space above retail. Several new buildings in the vicinity, like the Sunset Electric Building have preserved character facades under the Pike/Pine zoning overlay.

### CAL ANDERSON PARK

Cal Anderson Park defines the north edge of Pine Street near the site. The park is most active in the summer months, when crowds gather on the lawn. Views of the park will be a consideration for this project.





ZONING DATA

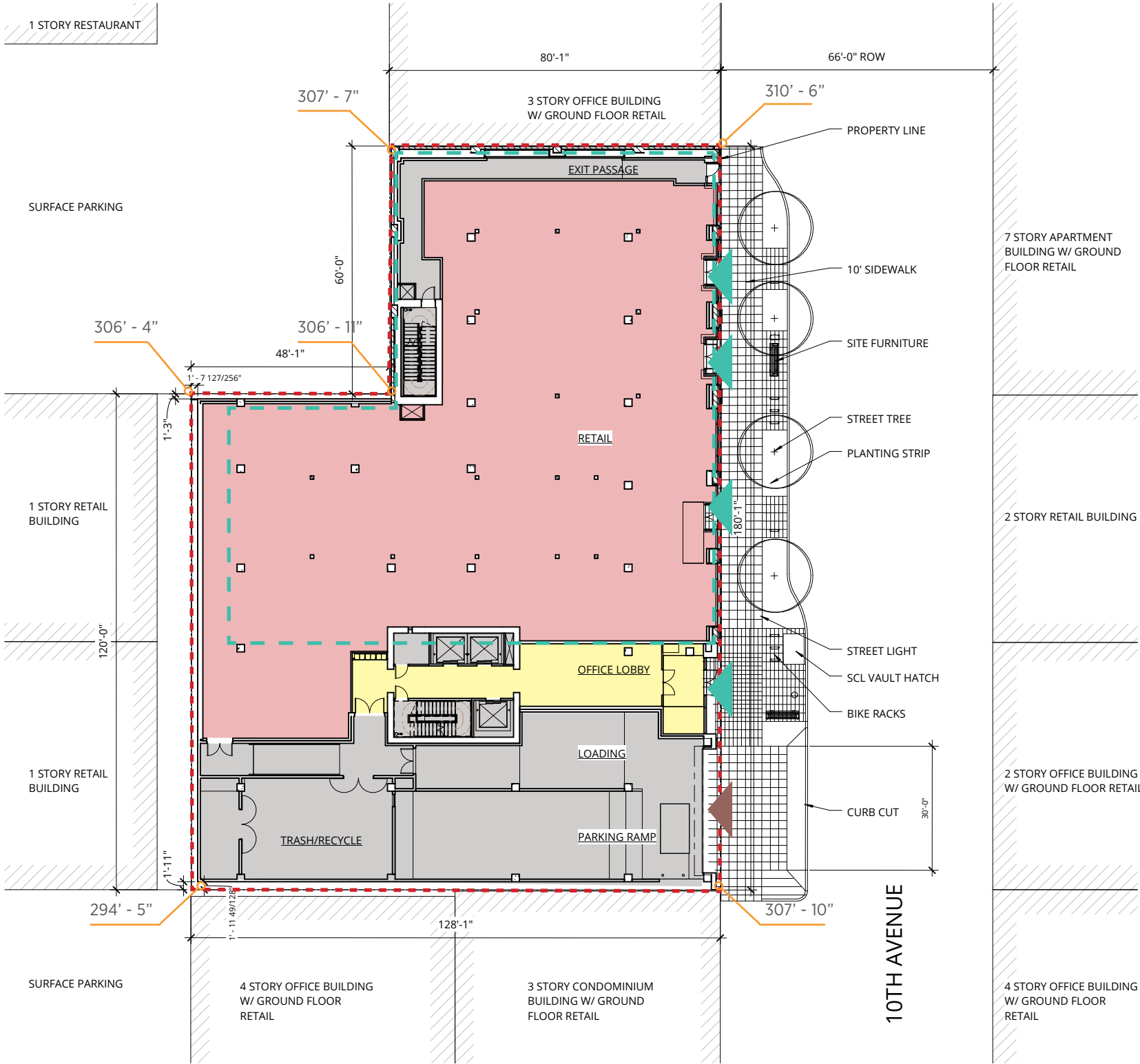
ZONING SUMMARY

LAND USE CODE SECTION	REQUIREMENT	COMMENTS
SITE LOCATION		KCAO Parcel # 6003500420 1525 11th Avenue, Seattle, WA
SITE ZONING OVERLAY ZONING		Zone: NC3P-65 Overlays: Pike/Pine Urban Village, Pike/Pine Conservation Overlay, Pike/Pine Conservation Overlay Conservation Core, Pedestrian Area
STREET CLASSIFICATION		1th Avenue is a Principal Pedestrian Street and is subject to the 10th And 11th Avenue Streetscape Concept Plan
SITE AREA		20,165 SF
23.47A.008 / 23.73.008 STREET LEVEL USES	No blank facade segments over 20' 60% transparency on street-facing facades Min 30' and average 15' depth for non-residential uses  Min ground level floor to floor height of 13' 80% of width must be approved ground floor uses	Complies - The proposed design includes no blank facade sections over 20' in width Complies - Transparency exceeds 60% Complies - Non-residential uses exceed an average of 30' deep and are never less than 15' deep, with a very small exception for the coffee kiosk Complies - Ground floor height is in excess of 13' Complies - 80% is in approved uses (see pre-submittal conference minutes for clarification regarding parking/loading area)
23.47A.012 / 23.73.014 STRUCTURE HEIGHT	Max height 65' per zone 10' additional allowed in overlay when a character structure is retained - departure required to occupy extra 10' with a non-residential use	Complies - Height is 65' + 10' per the additional height allowance described below Departure requested
23.47A.013 / 23.73.009 FLOOR AREA RATIO	Max FAR of 4.25 Floor area within a character structure exempt	Complies when combined with exceptions in overlay Brings total FAR below 4.25
23.73.012 WIDTH AND DEPTH LIMITS	Max width 128' Max depth 128'	Width separated from street by character structure exempt - complies Complies - Site is less than 128' deep
23.73.015 CHARACTER STRUCTURES	Must retain street facing facades 15' Setback required above retained facade Original floor to ceiling height must remain	Complies - Facade retained Complies - 15' setback maintained Complies - Floor to ceiling height maintained
23.47A.032 PARKING LOCATION & ACCESS	Parking access limited to a single 2-way curb cut For each permitted curb cut, street facing facades may have one garage door	Complies - One curb cut provided Complies - One garage door proposed
23.54.015 REQUIRED PARKING	No auto parking required in an Urban Center Bike parking required	Complies - Parking exceeds minimum Complies - Bike parking provided at level required
23.54.030 PARKING STANDARDS	Minimum widths of drive aisles required depending on stall size/configuration Maximum curb cut width if truck and auto access is combined is 30'	Complies - All aisles meet or exceed regulation width Complies - Curb cut is 30' wide
23.47A.035 LOADING BERTHS	3 berths required	Departure requested to allow 2 berths to be located in the garage and not meet head height standards.
23.54.040 SOLID WASTE	275 SF required	Complies - More than 275 SF provided



COMPOSITE SITE PLAN

- PEDESTRIAN ENTRY
- VEHICULAR ENTRY
- FOOTPRINT OF EXISTING HISTORIC LANDMARK





PRESERVATION PLAN PRESENTED  
AT EDG AND ACCEPTED BY  
LANMARKS PRESERVATION BOARD



EXISTING EAST ELEVATION

ORIGINAL HOLLOW CLAY TILE  
EXTERIOR COVERED W/ METAL SIDING  
TO BE REMOVED.

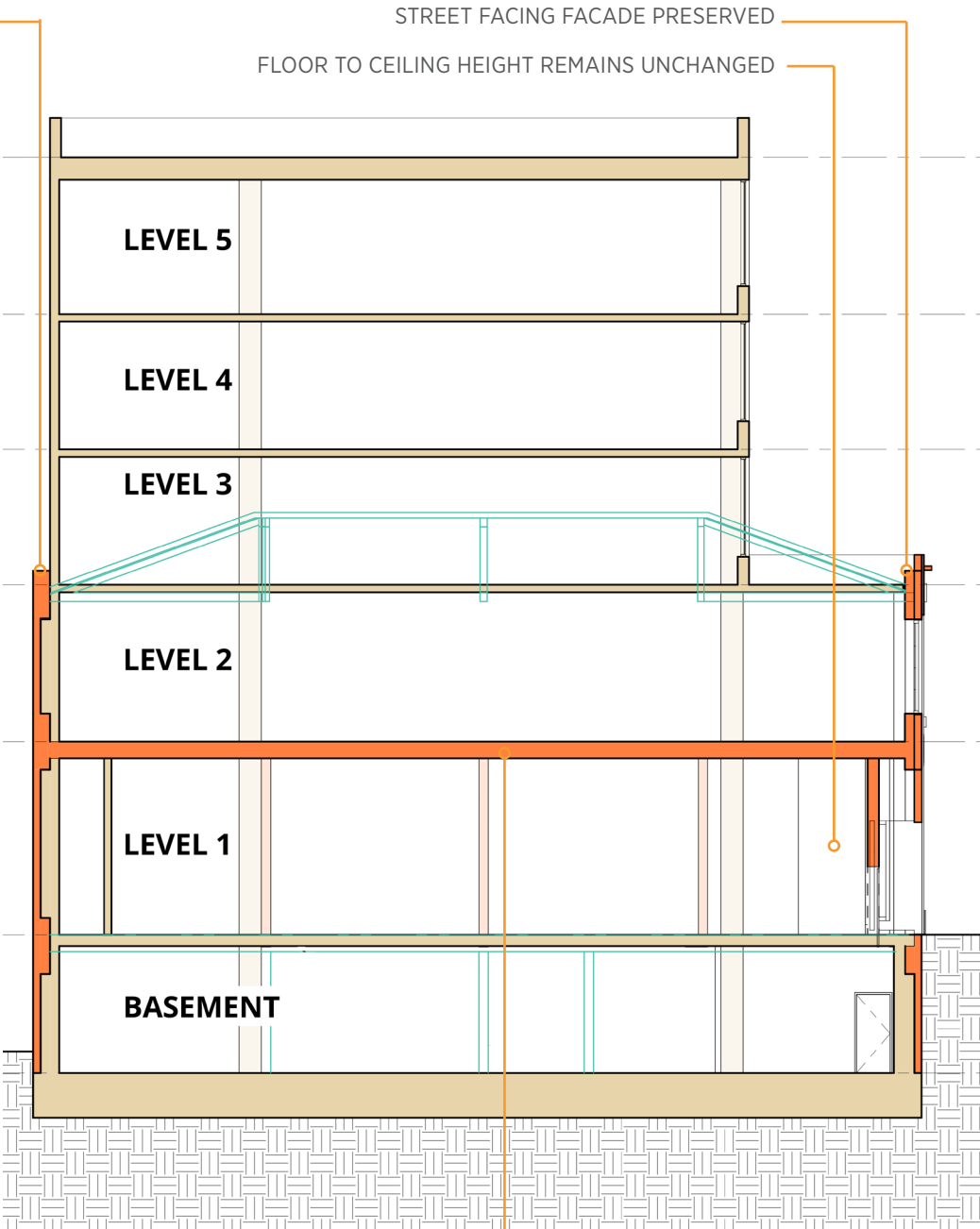
ORIGINAL BRICK PIERS TO BE CLEANED  
AND RE-POINTED

ORIGINAL STUCCO SPANDREL W/  
TILE DECORATION TO BE CLEANED  
AND RESTORED.

ONLY REMAINING ORIGINAL WINDOW. OTHER SECOND  
FLOOR WINDOWS TO BE REPLACED WITH REPLICA  
WINDOWS.

REAR FACADE  
ELEMENTS PRESERVED

STUCCO INFILL ELEMENTS AND  
DROPPED SOFFIT NOT ORIGINAL, TO BE  
REMOVED.



SECTION - PROPOSED PRESERVATION

SECOND FLOOR HEAVY TIMBER  
STRUCTURE REMOVED AND REINSTALLED

EXISTING ELEMENT TO REMAIN  
EXISTING ELEMENT TO BE REMOVED



APPROVED EDG OPTION

AREA BY FLOOR

PARKING	18,500 SF
LEVEL 1 (RETAIL)	19,600 SF
MEZZANINE (RETAIL)	1,800 SF
LEVEL 2 (OFFICE)	18,050 SF
LEVEL 3 (OFFICE)	15,350 SF
LEVEL 4 (OFFICE)	15,350 SF
LEVEL 5 (RESIDENTIAL)	15,350 SF
ROOF	1,200 SF
TOTAL	105,200 SF

OPPORTUNITIES

- Preferred by Landmarks Preservation Board
- Infill of south parking lot relates to prevailing building widths.
- Deeper setback than required by zoning over the landmark facade allows more breathing room.
- Simplicity of form relates to vernacular commercial style architecture.
- Divided garage/parking entries are compatible with the scale of streetscape elements on the block.

CONSTRAINTS

- 2 Garage doors on 11th Avenue (The departure required for this was not recommended to return by the DRB)



AXONOMETRIC



PERSPECTIVE NORTH UP 11TH AVENUE

SITE TRIANGLE NOT PROVIDED

25' PARKING ENTRY

12' LOADING ENTRY

OFFICE USE FROM LEVEL 2-5  
(DEPARTURE NOT TO HAVE  
RESIDENTIAL WITHIN 10'  
BONUS HEIGHT)



PERSPECTIVE WEST ACROSS 11TH AVENUE



EDG GUIDANCE RESPONSE

THEME 1: MASSING AND DESIGN

- 1 The Board supported elements of the massing, including the 10’ of additional height requested through the departure process and the setback above the character structure.
- 2 Provide a design where the new portion is either distinct from or similar to the landmark structure.
- 3 Consider designing the two lower floors of the new structure as an extension of the historic structure.
- 4 The Board would rather see a blank wall than a mural on the portion of the west facade that will not include any windows.
- 5 The layout of the ground floor is unresolved. Acknowledging public comment regarding the access and garage presence, the Board strongly encouraged a design that minimizes the loading area and the parking access.

THEME 2: RELATIONSHIP TO NEIGHBORING STRUCTURES

- 6 The relationship to the buildings along the rear west property line is acceptable.
- 7 The Board encouraged efforts to respect the existing windows on the residential structure to the south.
- 8 Given the open floor plan and office use, consider pulling the building back to provide more space for the windows near the property line.

THEME 3: STREETScape AND GROUND FLOOR

- 9 Design the entry to the office lobby to be more prominent. The design should be either similar to or distinct from the landmark structure entrance.
- 10 The entry design should be influenced by the entry of the landmark building if the design is to be similar to the landmark structure entrance.
- 11 Consider locating the office entry where the garage was shown at EDG.
- 12 Provide more vegetation on the curb protrusions into the street and a more thoughtful and dense landscape plan. Maintain the angled street parking.

THEME 4: SERVICE USES

- 13 Design the bike access circulation to be more user friendly.
- 14 Minimize the size of the garage doors; consider a smaller loading or smaller garage opening.





LANDMARKS PROCESS UPDATE

MEETINGS WITH THE LANDMARKS PRESERVATION BOARD TO DATE

3/31/2015	Architectural Review Committee Meeting #1
6/1/2015	Architectural Review Committee Meeting #2
12/11/2015	Architectural Review Committee Meeting #3
1/15/2016	Architectural Review Committee Meeting #4
3/11/2016	Architectural Review Committee Meeting #5
9/21/2016	Landmarks Preservation Board Full Board Briefing
12/16/2016	Architectural Review Committee Meeting #6

THE LANDMARKS PRESERVATION BOARD (LPB) HAS REVIEWED AND APPROVED OF THE MASSING AND DESIGN TO BE PRESENTED AT THE RECOMMENDATION MEETING

- 1 The LPB supported the corner windows on the south tower element.
- 2 The LPB appreciated the thoughtfulness of the art treatment on the garage door, and the placement of the garage door as far as possible from the landmark building. The committee noted that the lighting inside the garage should not create glare on the sidewalk.
- 3 The LPB supported the design of the preserved portion of the west elevation that will be presented at the recommendation meeting.

THE LPB PROVIDED FEEDBACK ON DETAILS OF THE LANDMARK BUILDING

- 1 The LPB supported the general signage direction shown in Recommendation meeting renderings.
- 2 The LPB appreciated the multipane wood restoration windows on the second floor.
- 3 The LPB supported the design of the recreated historic sconces on the landmark building.
- 4 The LPB indicated that storefront sills in the landmark building should match the existing condition.

NEXT STEPS WITH THE LANDMARKS PRESERVATION BOARD

After the Master Use Permit / SEPA decision has been published, the project will make a final presentation and document submittal to the Landmarks Preservation Board in order to obtain a Certificate of Approval.

Once the Certificate of Approval is obtained, the Master Use Permit will be completed, allowing the project to move forward into construction.





THEME 1: MASSING AND DESIGN

GUIDANCE

- 1 The Board supported elements of the massing, including the 10' of additional height requested through the departure process and the setback above the character structure.
- 2 Provide a design where the new portion is either distinct from or similar to the landmark structure.
- 3 Consider designing the two lower floors of the new structure as an extension of the historic structure.
- 4 The Board would rather see a blank wall than a mural on the portion of the west facade that will not include any windows.
- 5 The layout of the ground floor is unresolved. Acknowledging public comment regarding the access and garage presence, the Board strongly encouraged a design that minimizes the loading area and the parking access.



EDG DESIGN

RESPONSE

- 1 The additional 10' of height requested through the departure process and the 21' setback from the face of the landmark building to the face of the new construction above remain part of the design.
- 2 The new portion will feel appropriately distinct from the landmark building due to different materiality, bay widths, and height.
- 3 The office lobby can be seen as having a similar expression to the Kelly-Springfield bays, although the bay is wider and materiality is much different. The design team does not believe a double height expression is appropriate for the garage door, because attention should be focused on the office entry instead.
- 4 A mural is no longer shown on the west elevation. The wall will remain blank. Presumably the rancho bravo site will be developed in the future, obscuring the wall.
- 5 The ground floor layout of the parking/loading area has been revised to reduce the length of the facade devoted to auto oriented uses to the absolute minimum that will be safe. An artistic treatment will ensure that the garage door becomes an attractive part of the streetscape. This comment is primarily addressed in themes 3 and 4.



DRB DESIGN

DESIGN GUIDELINES

- 2 CS3.A.2 Contemporary Design  
CS3.III.i.b Visual Distinction between New and Old  
The curtain wall portion above the Kelly-Springfield will feel highly contemporary, while compatible with the historic building. Other portions of the building will relate more closely to historic neighbors, but will feature contemporary detailing.
- CS3A.3 Established Neighborhoods  
The style of the building, particularly the south element, is designed to relate to proportions and patterns on nearby buildings. The way the building is broken into pieces relates to the 60' lot module that defined the first wave of development in the neighborhood.
- CS3.IV.i Scale and Modulation  
New parts of the ground floor feature an articulation scale similar to the Kelly-Springfield, except for the garage entry. A departure to help the garage entry become more compatible was not supported at EDG. Overall building massing is carefully designed to relate to the scale of existing buildings.
- DC2.II.i.a Design Concept  
The design concept blends contrast and transition. The element over the character structure is clearly contrasting, while drawing some fundamental rhythms from the context. The south element is more of a transition piece, almost feeling like a separate infill building with punched windows similar to on nearby older buildings.
- 3 CS3.IV.i Scale and Modulation  
New parts of the ground floor feature an articulation scale similar to the Kelly-Springfield, except for the garage entry. A departure to help the garage entry become more compatible was not supported at EDG.
- 4 DC2.B.1 Facade Composition  
Facades are proportioned to reflect the internal structure of the building, as are the auto row buildings that define the neighborhood. Rear and side facades feature appropriate materials and are well proportioned.
- 5 DC1.B.1 Access Location and Design  
The parking/loading entrance is located on a major pedestrian street, because the project only faces one right of way. Safety features will be incorporated. The sidewalk features necessary width and pedestrian amenities.



## PERSPECTIVE LOOKING SOUTH DOWN 11TH AVENUE

1 The additional 10' of height requested through the departure process and the 21' setback from the face of the landmark building to the face of the new construction above remain part of the design.

2 The southern "tower element" is clearly distinct from the landmarked Kelly-Springfield Building in materiality, bay widths, and height.

The bar element, directly above the Kelly-Springfield, also has its own personality, designed to be as simple and minimal as possible in order to provide a quiet backdrop behind the landmark.





EAST ELEVATION RENDERING

3 The office lobby can be seen as having a similar expression to the Kelly-Springfield bays, although the bay is wider and materiality is much different. The design team does not believe a double height expression is appropriate for the garage door, because attention should be focused on the office entry instead.





PERSPECTIVE LOOKING SOUTHEAST ACROSS THE RANCHO BRAVO PARKING LOT

4 A mural is no longer shown on the west elevation. Presumably the Rancho Bravo site will be developed in the future, obscuring the wall.

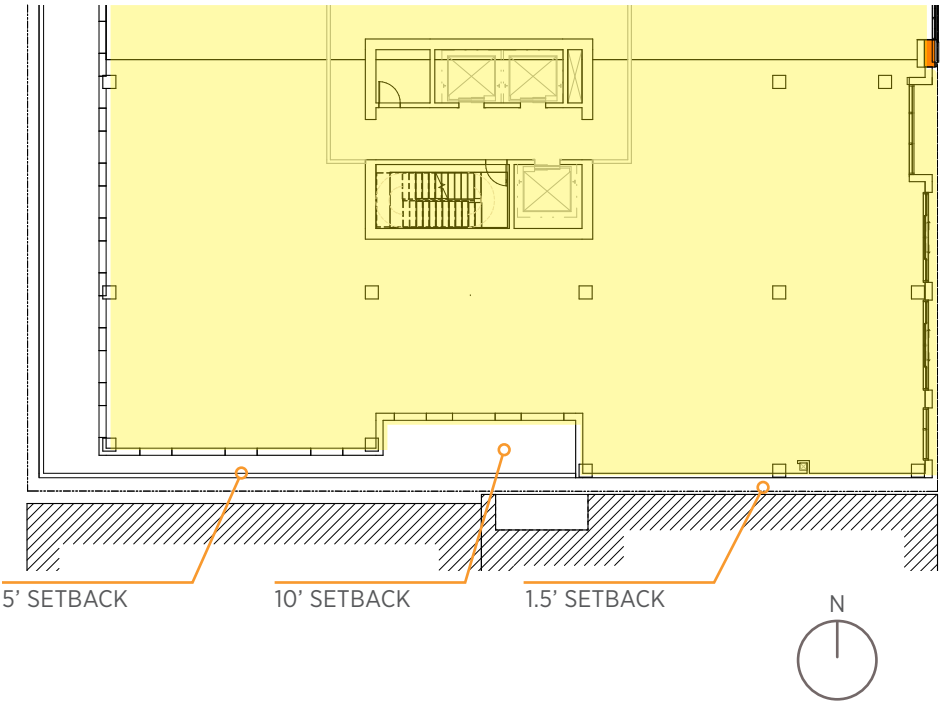
The blank wall follows guideline DC2-B-2 by featuring historic elements at pedestrian level. Corrugated metal panels provide texture and scale on the wall.





THEME 2: RELATIONSHIP TO NEIGHBORING STRUCTURES

GUIDANCE	RESPONSE	DESIGN GUIDELINES
<p>6 The relationship to the buildings along the rear west property line is acceptable.</p> <p>7 Make every effort to respect the existing windows on the residential structure to the south.</p> <p>8 Given the open floor plan and office use, consider pulling the building back to provide more space for the windows near the property line.</p>	<p>6 The relationship of the new building to adjacent structures along the rear west property lines remains consistent with the EDG presentation.</p> <p>7 Floor plates have been revised to provide a 5' setback from the property line at the locations of windows on adjacent buildings. Windows on the new building have been offset from windows in the Monique Lofts in order to minimize views directly into residential units.</p> <p>8 Windows in the Monique Lofts' light well will be at least 10' from the new building. The smaller windows on the property line will be 5' from the new building. Note that a 5' setback from the property line is consistent with the decisions made by the Monique Lofts developers when the condominium conversion was completed (The light well steps back 5').</p>	<p>6 <u>CS2.D.5</u> <u>Respect for Adjacent Sites</u> Buildings to the west are set back substantially from the proposed project. The new building will set back 10' from the property line along the west.</p> <p>7 <u>CS2.D.5</u> <u>Respect for Adjacent Sites</u> The south elevation of the building has been redesigned in order to preserve light and air as well as provide privacy to users of both structures adjoining the project to the south. Much of the building is set back from 11th Ave on upper levels, enhancing privacy for residents of Sunset Electric.</p> <p>8 <u>DC2.A.1</u> <u>Site Characteristics and Uses</u> The building massing is designed to relate to the scale of the character structure and surrounding buildings, while giving the character structure sufficient space to maintain its integrity. The massing is also configured to allow usable office floorplates.</p>



EDG PLAN, WITH SOME WINDOWS ON THE MONIQUE LOFTS BLOCKED



MONIQUE LOFTS & 1016 PIKE

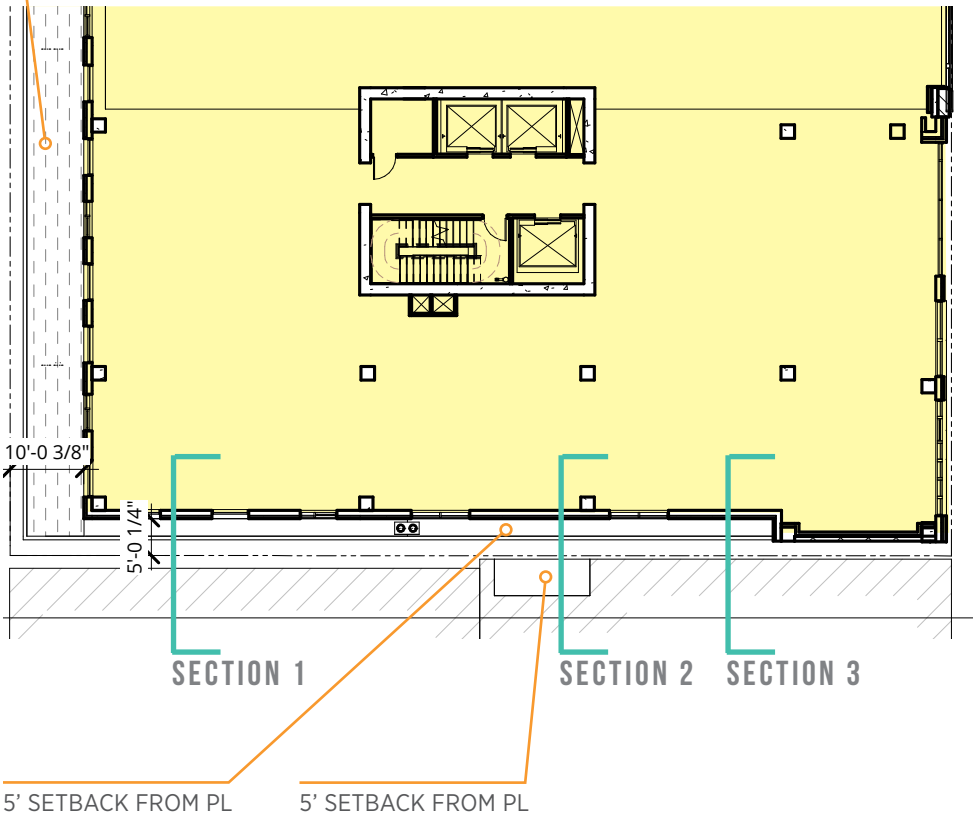
LIGHT WELL ON THE MONIQUE LOFTS, FEATURING A 5' SETBACK FROM THE PROPERTY LINE

WINDOWS DIRECTLY ON THE PROPERTY LINE

3' SETBACK

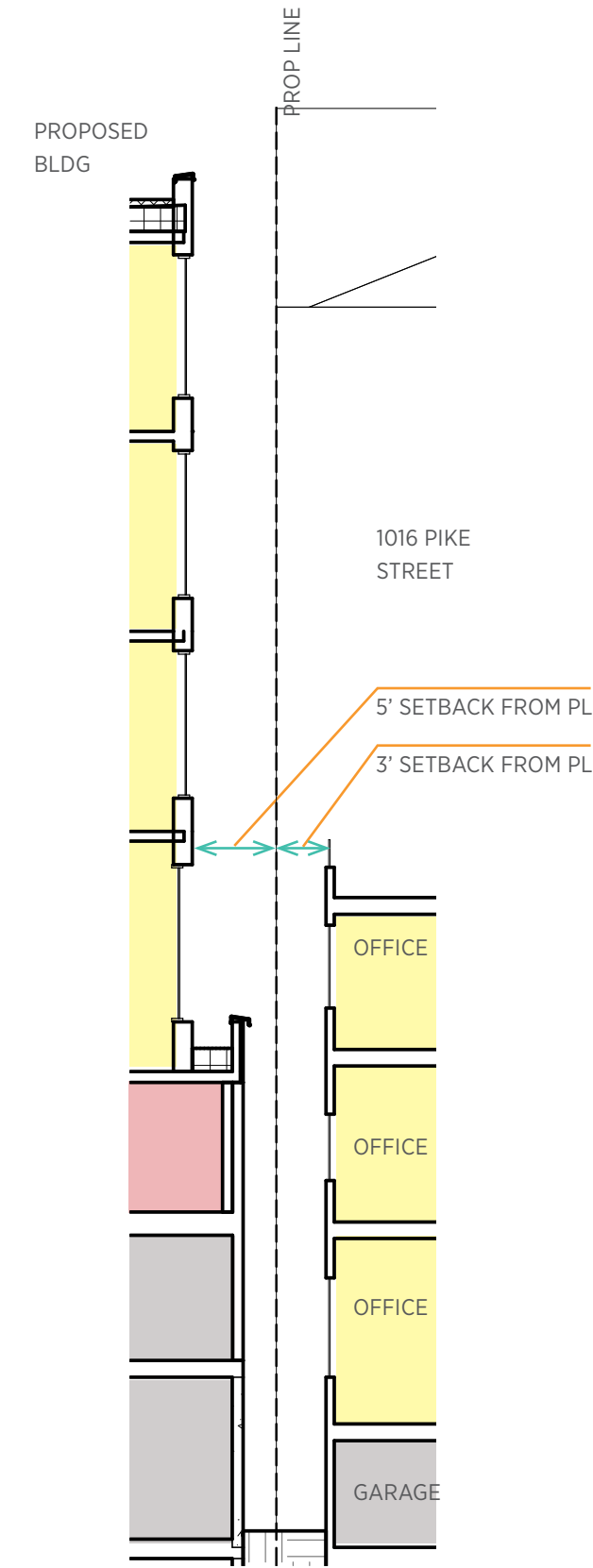


6 The relationship of the new building to adjacent structures along the rear west property lines remains consistent with the EDG presentation.

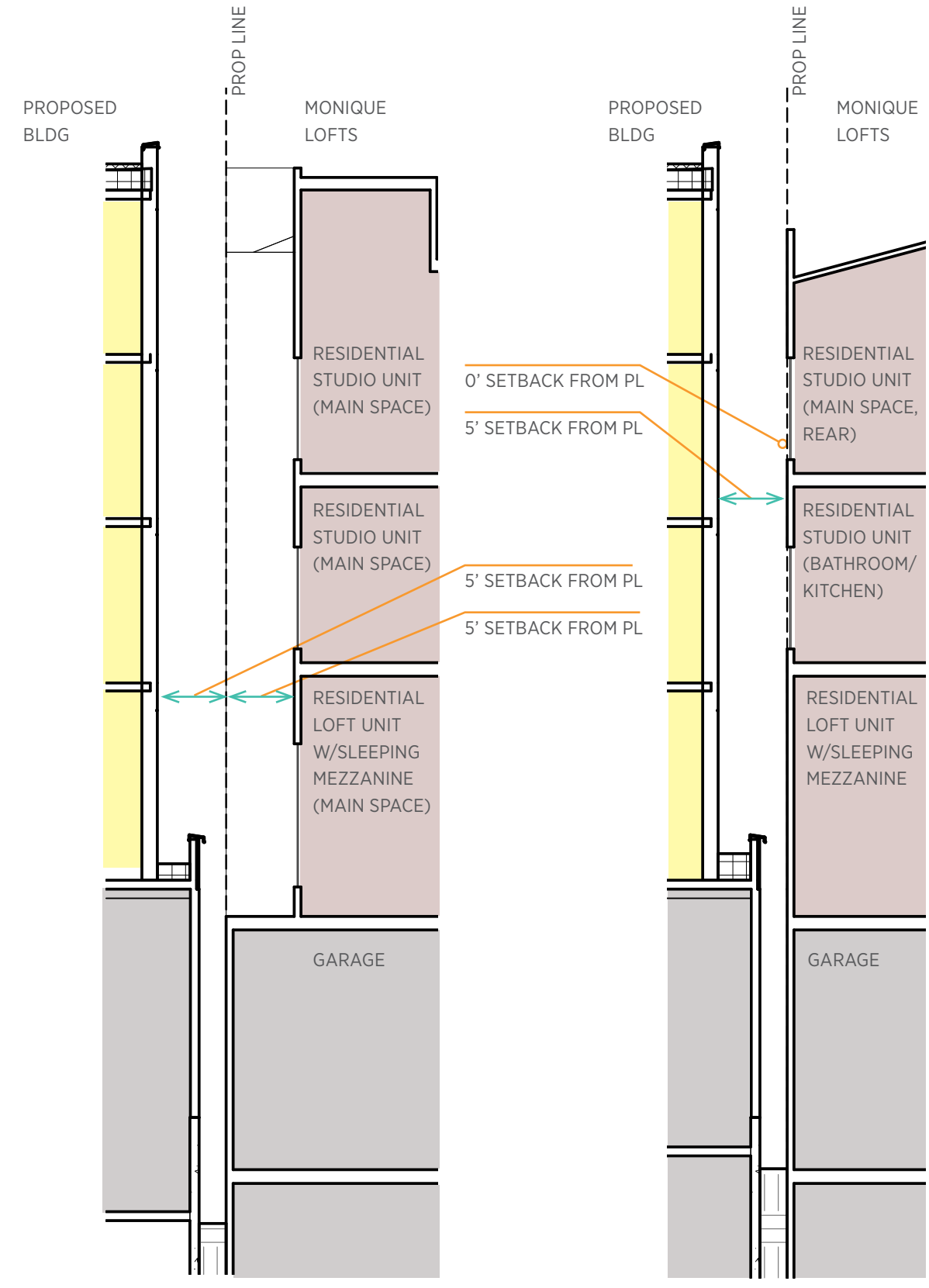


**PARTIAL UPPER LEVEL PLAN SHOWING**  
RELATIONSHIP WITH NEIGHBORING BLDGS

8 Windows in the Monique Lofts' light well will be at least 10' from the new building. The smaller windows on the property line will be 5' from the new building. Note that a 5' setback from the property line is consistent with the decisions made by the Monique Lofts developers when the condominium conversion was completed (The light well steps back 5')



**SECTION 1**  
PROPOSED BLDG @ 1016 PIKE



**SECTION 2**  
PROPOSED BLDG @ MONIQUE LIGHT WELL

**SECTION 3**  
PROPOSED BLDG @ MONIQUE WDWS



8 Windows on the new building have been offset from windows in the Monique Lofts in order to minimize views directly into residential units.



SOUTH ELEVATION / WINDOW STUDY



**PAGE INTENTIONALLY  
LEFT BLANK**



THEME 3: STREETScape AND GROUND FLOOR

GUIDANCE

- 9 Design the entry to the office lobby to be more prominent. the design should be either distinct or different from the landmark structure entrance.
- 10 The entry design should be influenced by the entry of the landmark building if the design is to be similar to the landmark structure entrance.
- 11 Consider locating the office entry where the garage was shown at EDG.
- 12 Provide more vegetation on the curb protrusions into the street and a more thoughtful and dense landscape plan. Maintain the angled street parking.



EDG RENDER SHOWING STREETScape AND OFFICE ENTRY  
LEGACY CAPITOL HILL / PROJECT #3023226  
RECOMMENDATION MEETING / 1/25/2017

RESPONSE

- 9 The revised office entry is quite prominent as the only location where the windows above break from their horizontal emphasis and turn down to the street. An elegant lighting strategy, custom canopy, and design of plantings and paving, all work together to further emphasize the importance of this main entry into the building.
- 10 The office lobby entry is designed to appear clearly distinct from the landmark. While the proportions pull cues from the landmark, the materiality and width of the bay will provide appropriate differentiation.
- 11 The lobby and garage entry locations shown avoid locating the garage entry adjacent to the landmark building (preferred by Landmarks Preservation Board) and groups service uses with services on the Monique lofts towards the middle of the block, away from intersections, preserving existing traffic patterns.
- 12 More trees and vegetation have been added to the streetscape. SDOT, via the SIP process, will not allow angled parking. The revised design, with the exception of angled parking, is consistent with the 10th /11th Avenue Streetscape Concept Plan.



DRB RENDER SHOWING STREETScape AND OFFICE ENTRY

DESIGN GUIDELINES

- 9 PL3.A.1 Design Objectives  
The primary office lobby is distinguished by an all glass entry door and canopy. The lobby is almost totally visible from the street, through a glazed vestibule. Retail entries are located in the character structure.
- CS3.III.i.b Visual Distinction between New and Old  
The curtain wall portion above the Kelly-Springfield will feel highly contemporary, while compatible with the historic building. Other portions of the building will relate more closely to historic neighbors, but will feature contemporary design and materials.
- CS3.IV.i Scale and Modulation  
New parts of the ground floor feature an articulation scale similar to the Kelly-Springfield, except for the garage entry. A departure to help the garage entry become more compatible was not supported at EDG. Overall building massing is carefully designed to relate to the scale of existing buildings.
- 10 CS3.I.iii Opening Proportions  
Fenestration proportions on new parts of the building relate to proportions on the Kelly-Springfield, which is consistent with the overall neighborhood patterns. The first floor windows exceed 15'. Second floor windows take their cues from the Kelly-Springfield window heights & datum lines.
- CS3.IV.i Scale and Modulation  
New parts of the ground floor feature an articulation scale similar to the Kelly-Springfield, except for the garage entry. A departure to help the garage entry become more compatible was not supported at EDG. Overall building massing is carefully designed to relate to the scale of existing buildings.
- 11 PL3.A.1 Design Objectives  
The primary office lobby is distinguished by a monumental door and canopy. The lobby is almost totally visible to the street, through a glazed vestibule. Retail entries are located in the character structure.
- 12 CS2.B.2 Connection to the Street  
Multiple entries from the sidewalk, large display windows, and a coffee window by the lobby all create connections to the street. Plentiful glazing, operable windows, and terraces on upper levels allow building users to connect to the streetscape.



## PERSPECTIVE LOOKING NORTH UP 11TH AVE



SIGNAGE PRECEDENT

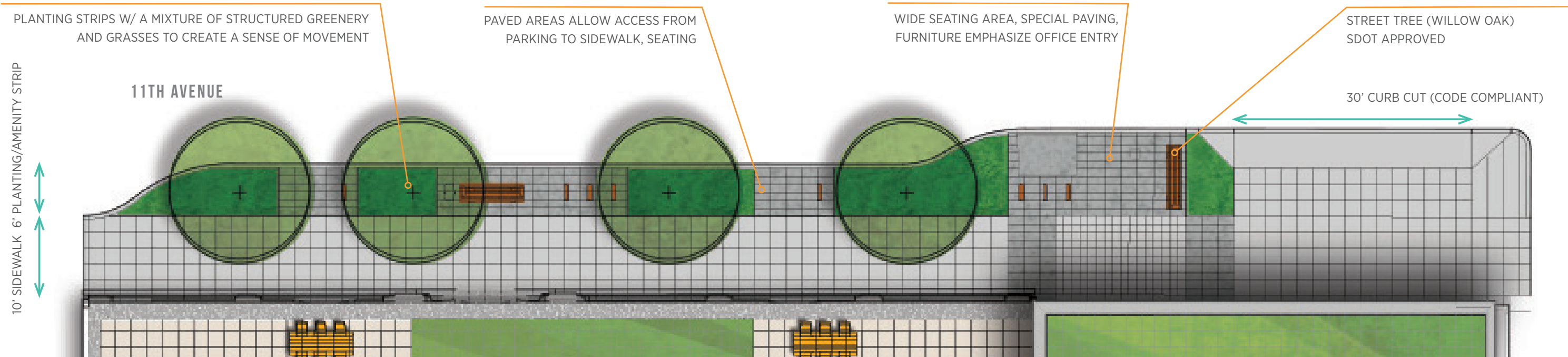
- 9** The revised office entry is quite prominent as the only location where the windows above break from their horizontal emphasis and turn down to the street. An elegant lighting strategy, custom canopy, and design of plantings and paving, all work together to further emphasize the importance of this main entry into the building.
- 10** The office lobby entry is designed to appear clearly distinct from the landmark. While the proportions pull cues from the landmark, the materiality and width of the bay will provide appropriate differentiation.
- 12** More trees and vegetation have been added to the streetscape. SDOT, via the SIP process, will not allow angled parking. The revised design, with the exception of angled parking, is consistent with the 10th /11th Avenue Streetscape Concept Plan.

GARAGE DOOR ART INSTALLATION



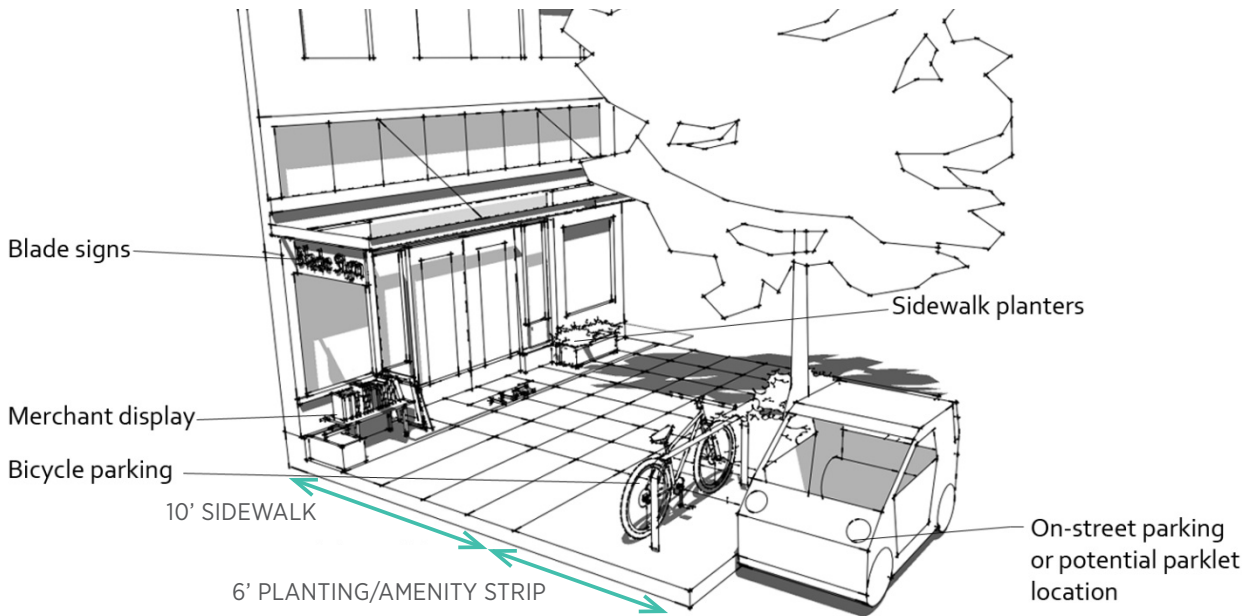


ITEMIZED RESPONSE TO EDG

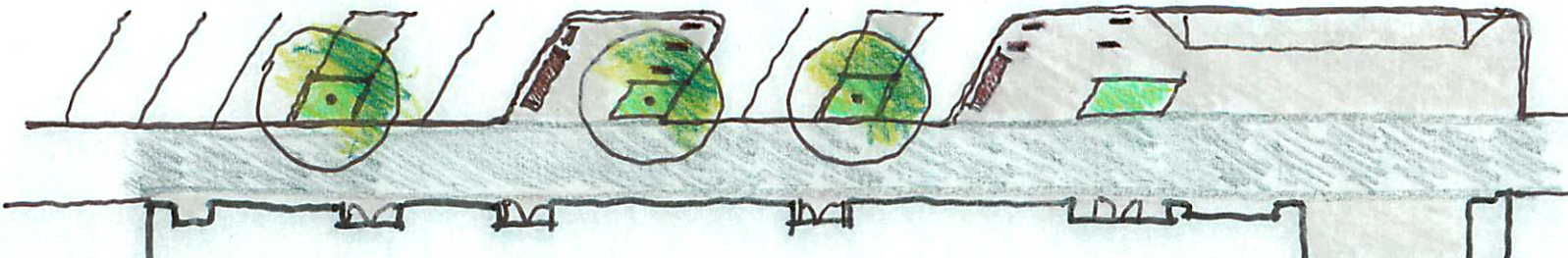


PROPOSED STREETScape DESIGN RESPONDING TO EDG FEEDBACK

SEE PAGE 35 FOR COMPLETE STREETScape AND LANDSCAPE DESIGN



SIDEWALK SECTION - 10TH AND 11TH AVENUE STREETScape CONCEPT PLAN  
PER SDOT DIRECTION, THIS SECTION IS THE BASIS OF THE PROPOSED STREETScape DESIGN



STREETScape CONCEPT PRESENTED AT EDG





PERSPECTIVE OF OFFICE LOBBY ENTRANCE AND TRANSITION TO LANDMARK BUILDING

CUSTOM ENTRY SIGN MOUNTED ON GLASS & METAL CANOPY

GARAGE DOOR ART INSTALLATION

ALL GLASS ENTRY



NEW WOOD WINDOWS

NEW CURTAIN WALL SYSTEM

REPLICA HISTORIC SCONCES

RETAIL BLADE SIGNAGE

SPECIAL PAVING PATTERN AND PLANTINGS EMPHASIZES ENTRY



ITEMIZED RESPONSE TO EDG

PARTIAL EAST ELEVATION



11 The lobby and garage entry locations shown avoid locating the garage entry adjacent to the landmark building (preferred by Landmarks Preservation Board).

PARTIAL EAST ELEVATION RENDERING



11 The lobby and garage entry locations shown group service functions on the Monique Lofts and the proposed development towards the middle of the block, minimizing their impact.

Proposed garage entry (same location as current parking lot entry)

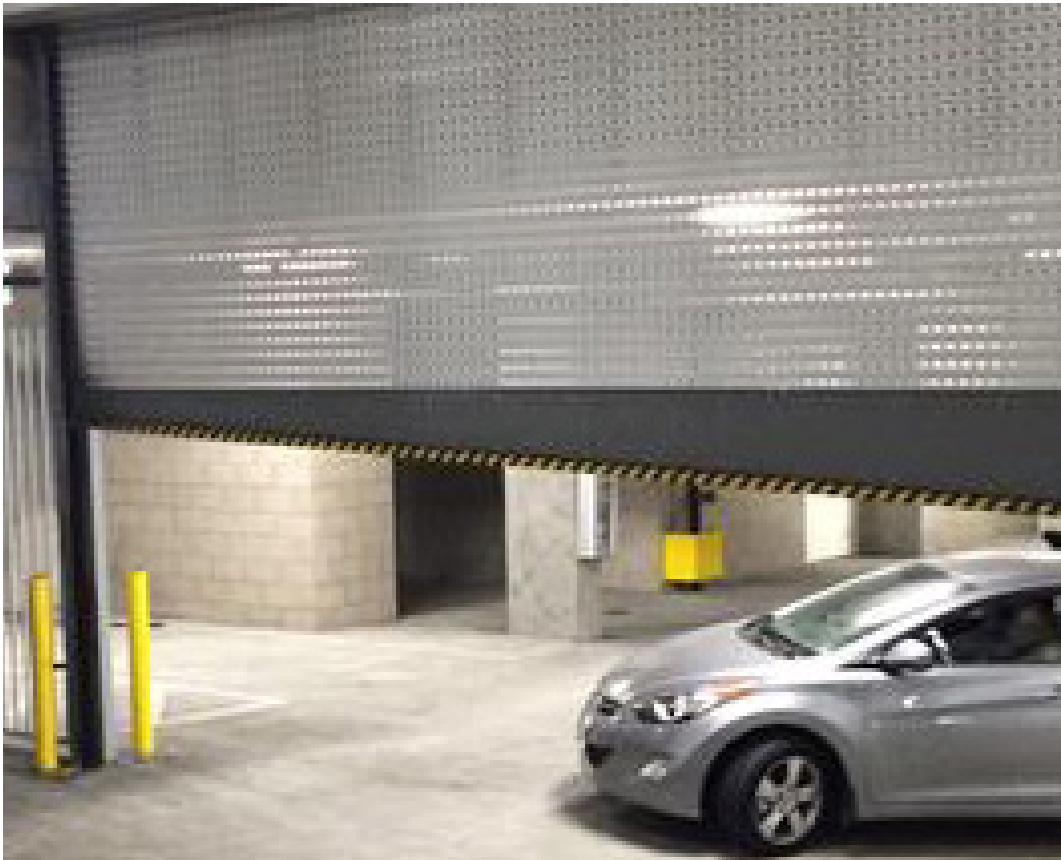
Monique Lofts garage entry

Service uses grouped towards the center of the block



SATELLITE VIEW OF 11TH AVENUE BETWEEN PIKE AND PINE



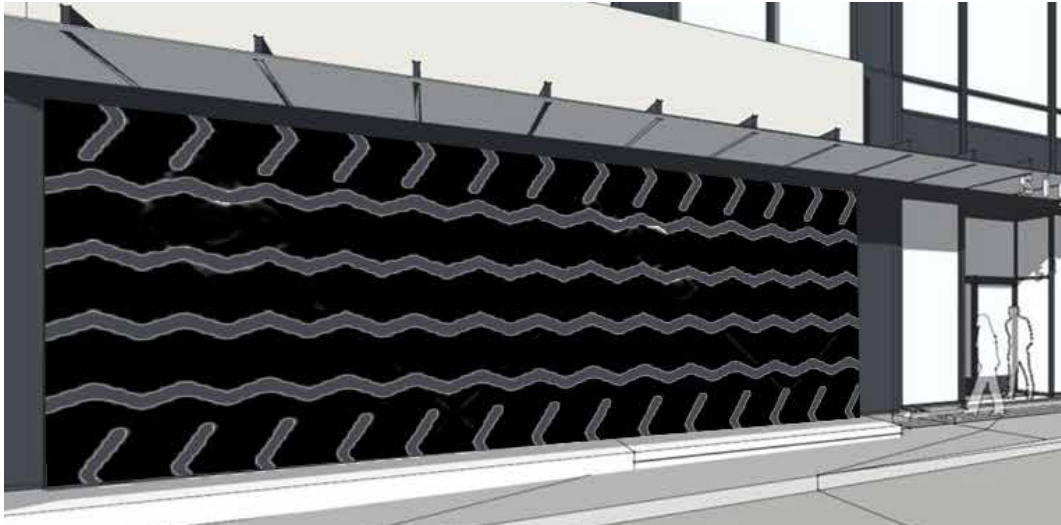


COILING PERFORATED GARAGE DOOR W/ SLOPED BOTTOM, OF THE TYPE TO BE PAINTED WITH A DECORATIVE TIRE-BASED PATTERN

1 3/8" X 3/8" PERFORATIONS 2 1/2" O.C.

A technical diagram showing a cross-section of the perforated pattern. It consists of two rows of rectangular perforations. The top row is offset from the bottom row. The dimensions are specified as 1 3/8" x 3/8" for the perforations and 2 1/2" O.C. (on center) for the spacing.

RENDERING SHOWING GARAGE DOOR W/ PAINTED ART INSTALLATION



TIRE TREAD CONCEPT SKETCH FROM ARTIST RETAINED BY OWNER



EXAMPLES OF WORK BY ARTIST (ELECTRIC COFFIN COMPANY)





THEME 4: SERVICE USES

GUIDANCE

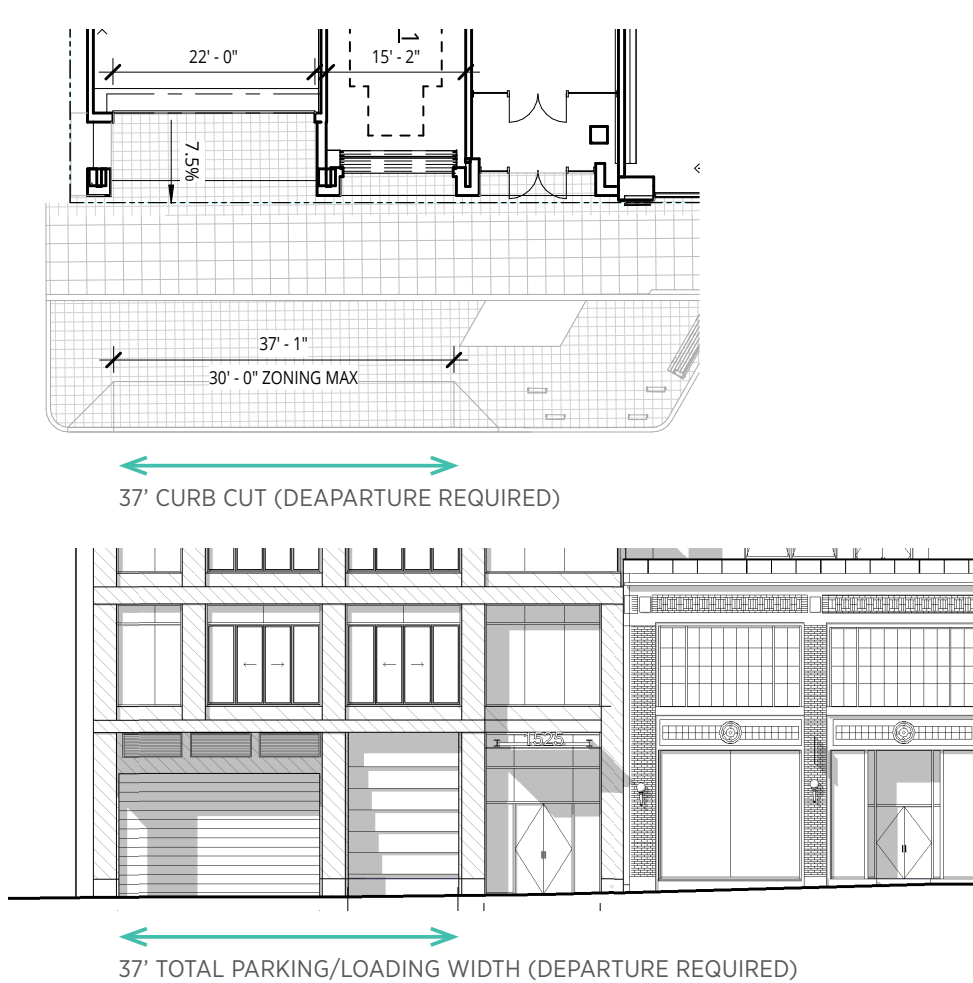
- 13 Minimize the size of the garage doors; consider a smaller loading or smaller garage opening.
- 14 Design the bike access circulation to be more user friendly.

RESPONSE

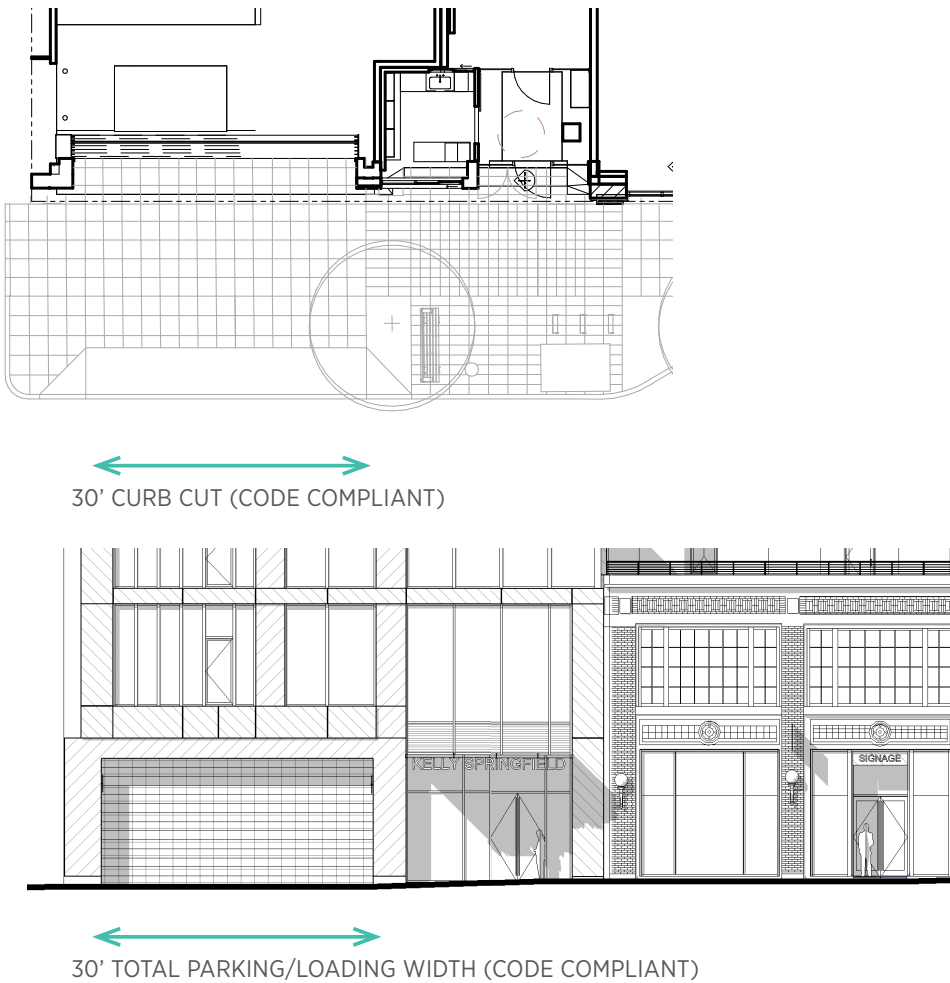
- 13 The departure to allow two garage doors presented at EDG has been eliminated. Parking and loading have been combined into one garage door, with a smaller total width than was presented at EDG. The parking/loading entry door has been minimized to the narrowest dimension possible while still providing safe access for parking and loading.
- 14 The bicycle circulation is user friendly. Two separate methods of access are provided—through the lobby via freight elevator and via the parking ramp. Showers, lockers, and bike ramps are located below grade to centralize bike functions in the building. For public bike usage, bike racks are located on the street.

DESIGN GUIDELINES

- 13 DC1.C.2 Visual Impacts  
A garage door and curb cut are the only visual impacts of parking and loading on the site. The garage door has been squeezed to the minimum dimension to safely accommodate parking and loading.
- 14 PL4.B.3 Bike Facilities  
Showers, lockers, and bike ramps are located in the below-grade garage, accessible by the parking ramp or by elevator from the lobby. Additional bike racks are incorporated into the streetscape design.



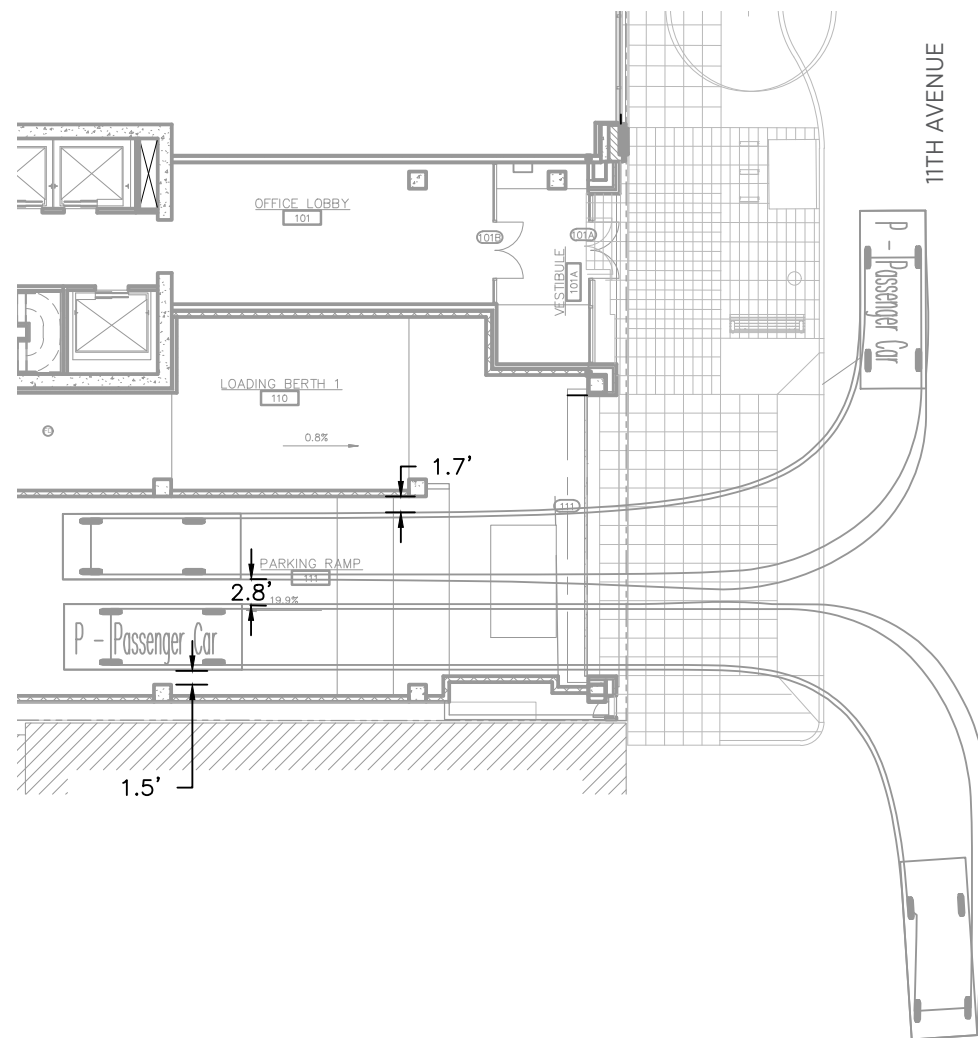
PARKING/LOADING DESIGN AT EDG



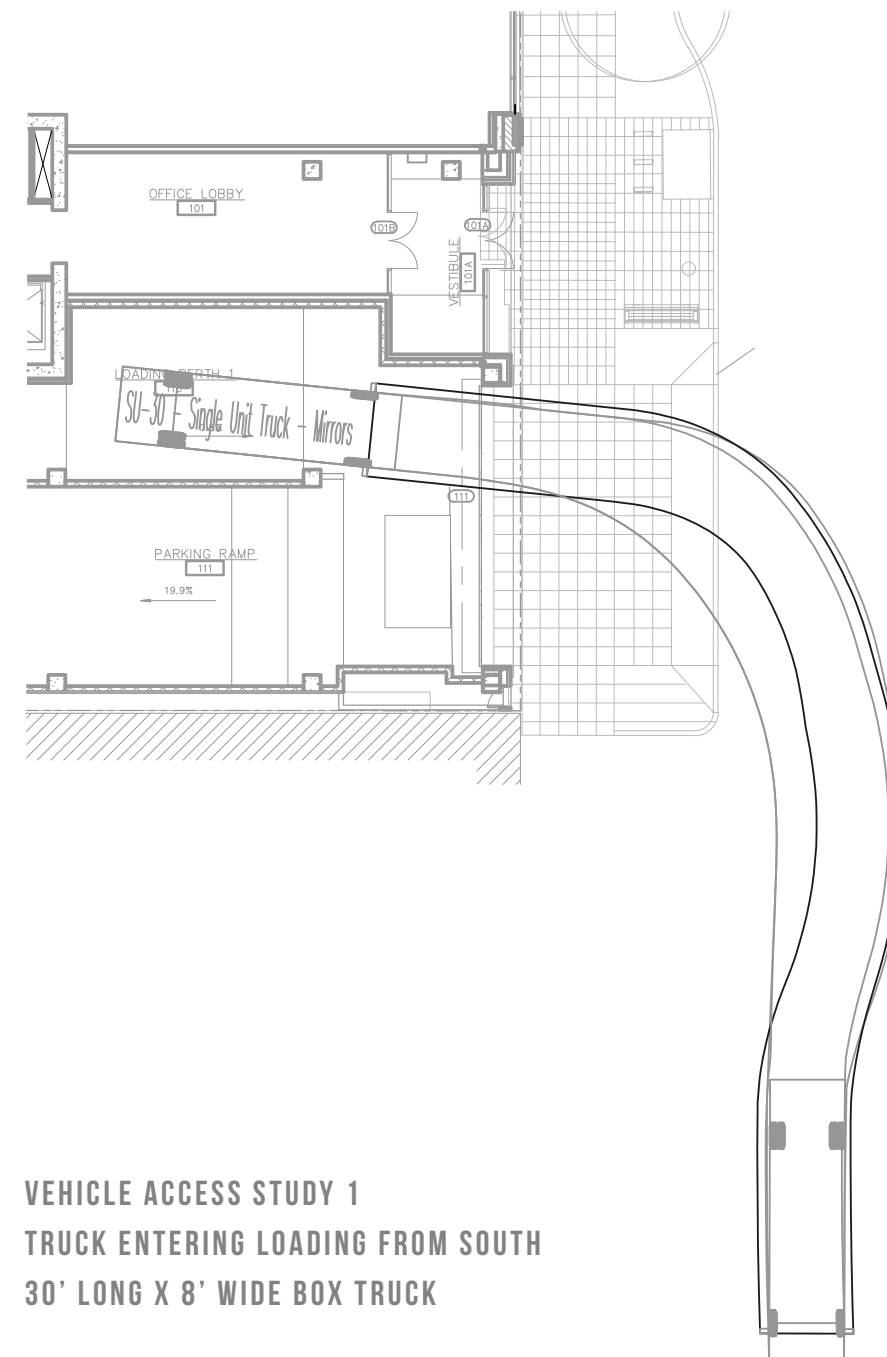
PARKING/LOADING DESIGN AT RECOMMENDATION



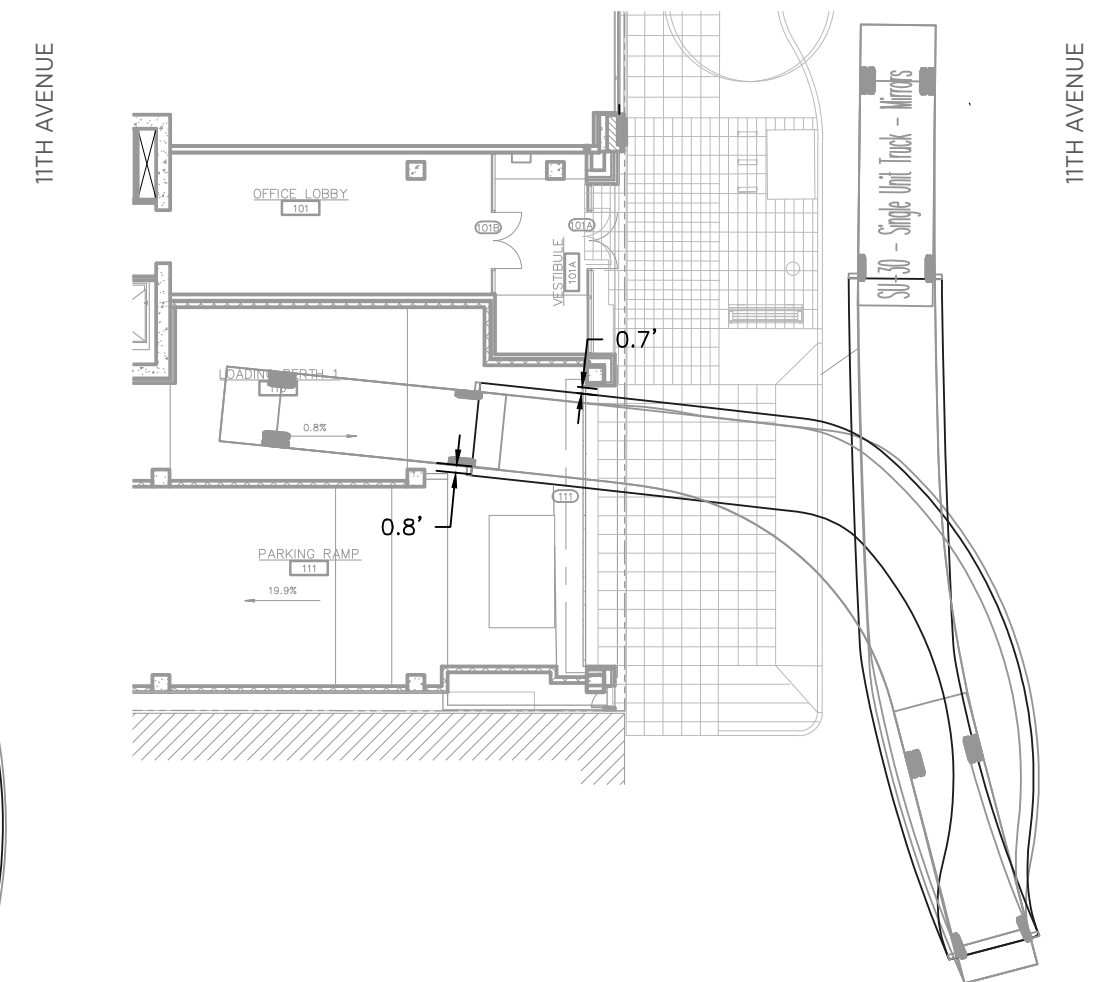
- 13** The parking/loading entry door has been minimized to the narrowest dimension possible while still providing safe access for parking and loading.



**VEHICLE ACCESS STUDY 1**  
PASSENGER CAR ENTERING/EXITING PARKING RAMP  
19' LONG X 7' WIDE SEDAN



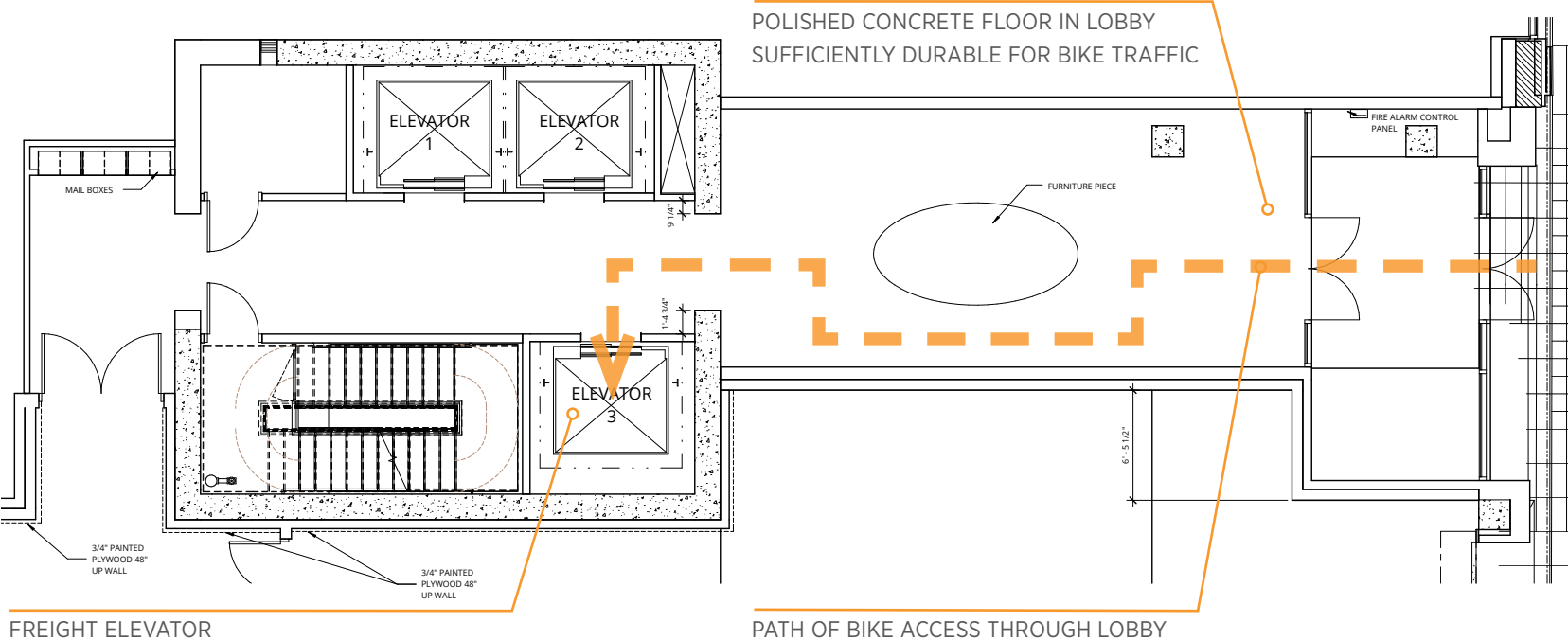
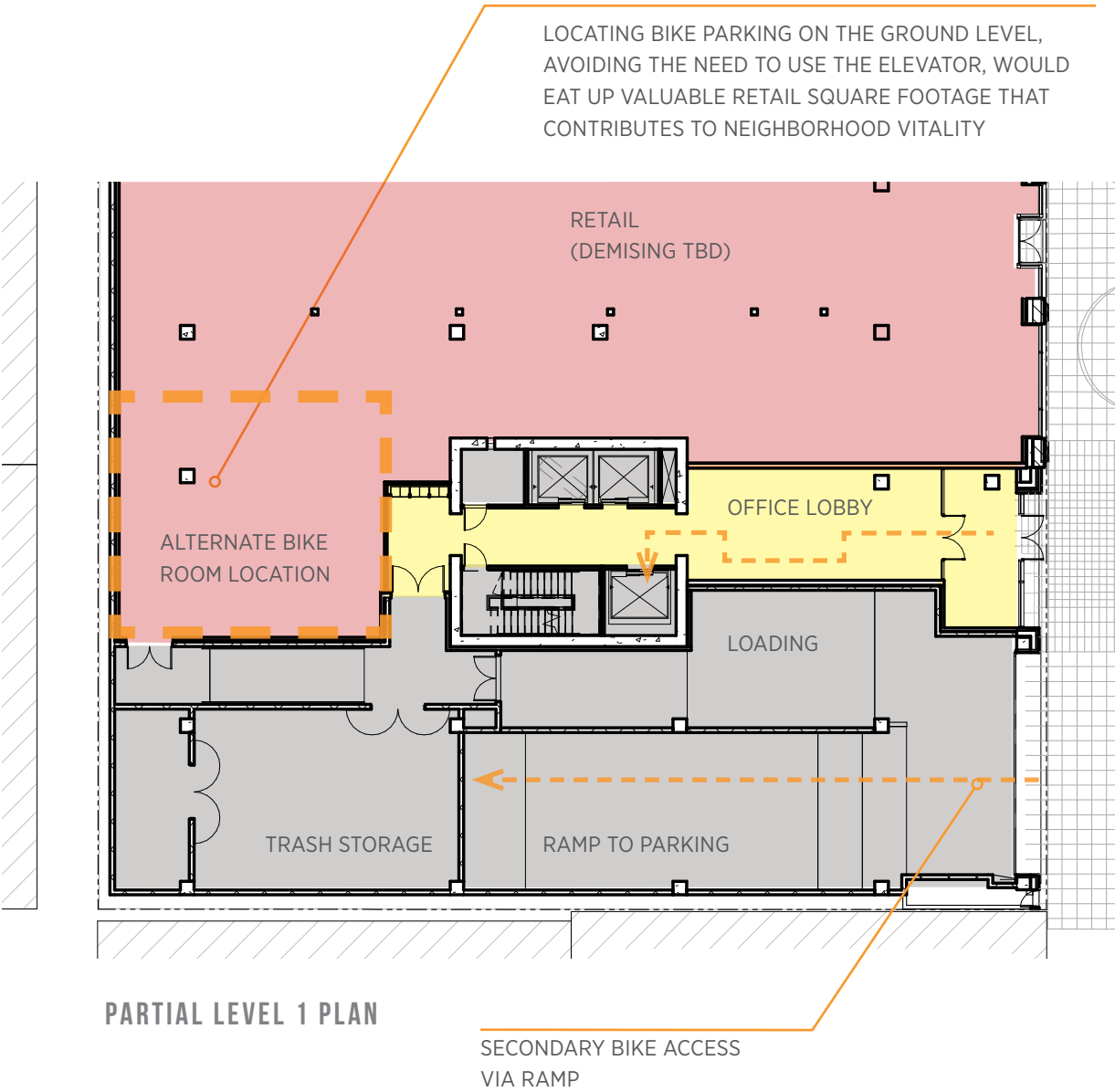
**VEHICLE ACCESS STUDY 1**  
TRUCK ENTERING LOADING FROM SOUTH  
30' LONG X 8' WIDE BOX TRUCK



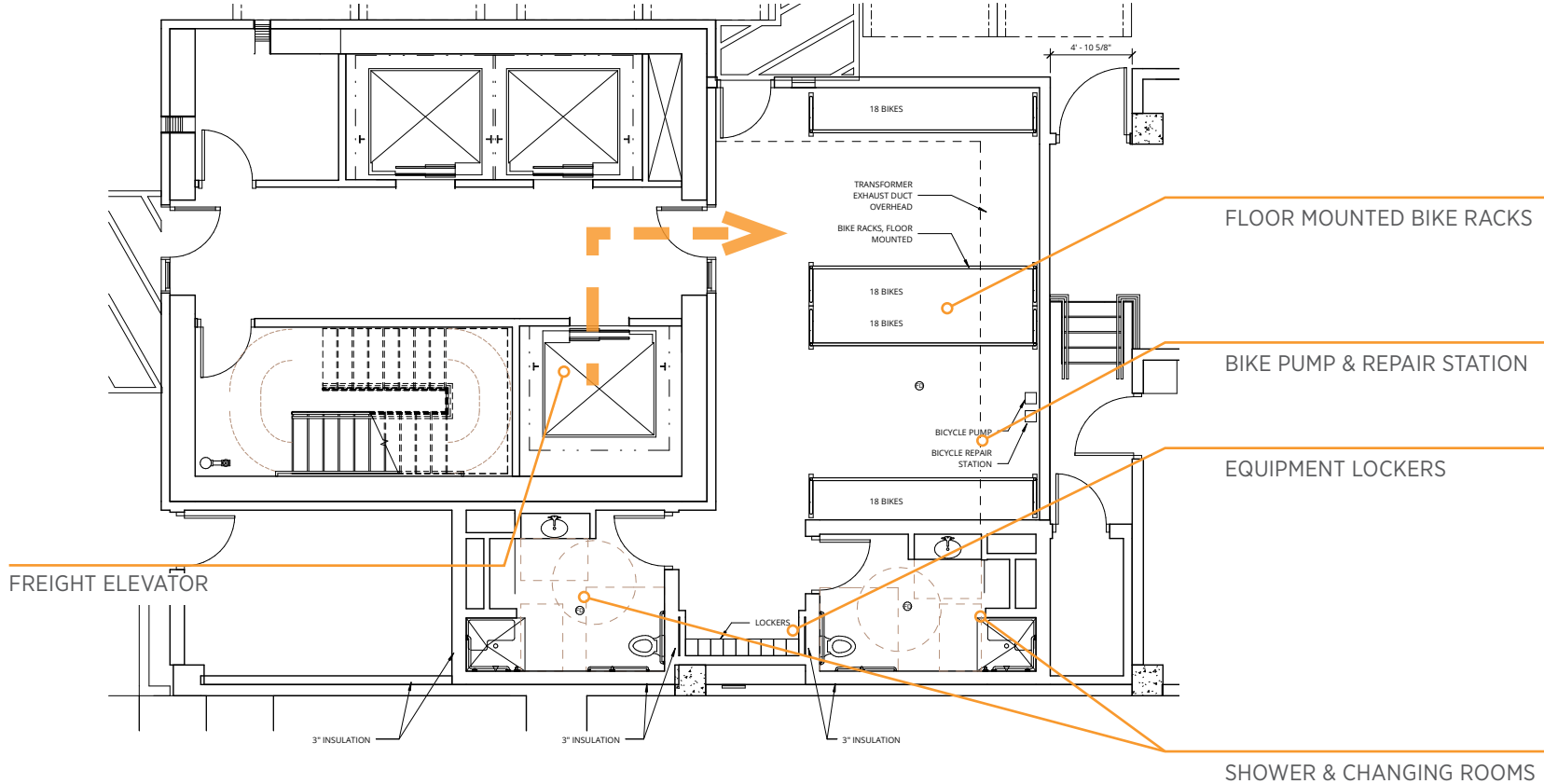
**VEHICLE ACCESS STUDY 2**  
TRUCK ENTERING LOADING FROM SOUTH  
30' LONG X 8' WIDE BOX TRUCK



14 The bicycle circulation is user friendly. Two separate methods of access are provided—through the lobby via freight elevator and via the parking ramp. Showers, lockers, and bike ramps are located below grade to centralize bike functions in the building. For public bike usage, bike racks are located on the street.



LEVEL 1 BICYCLE ACCESS PLAN



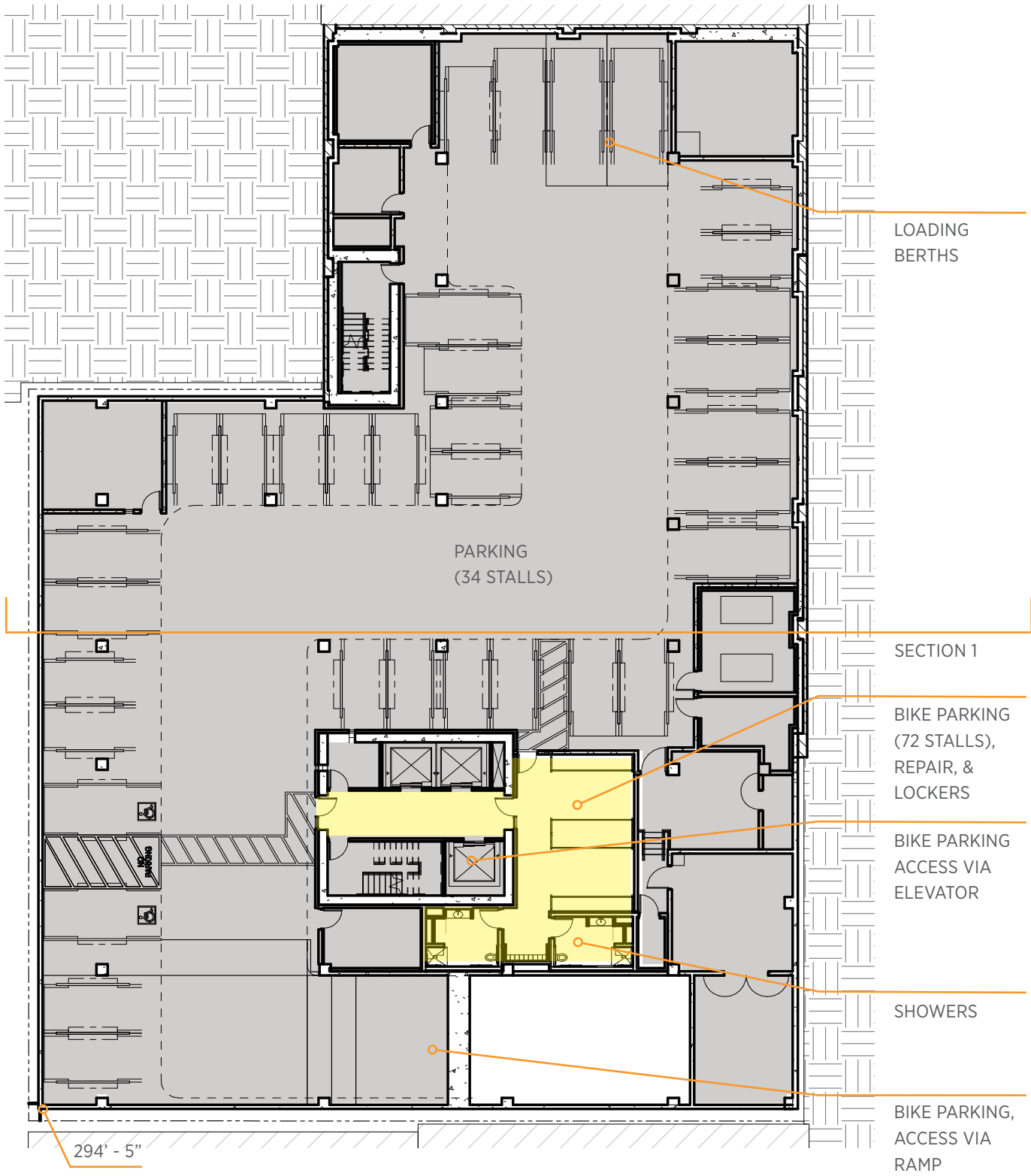
LEVEL P1 BICYCLE ACCESS PLAN



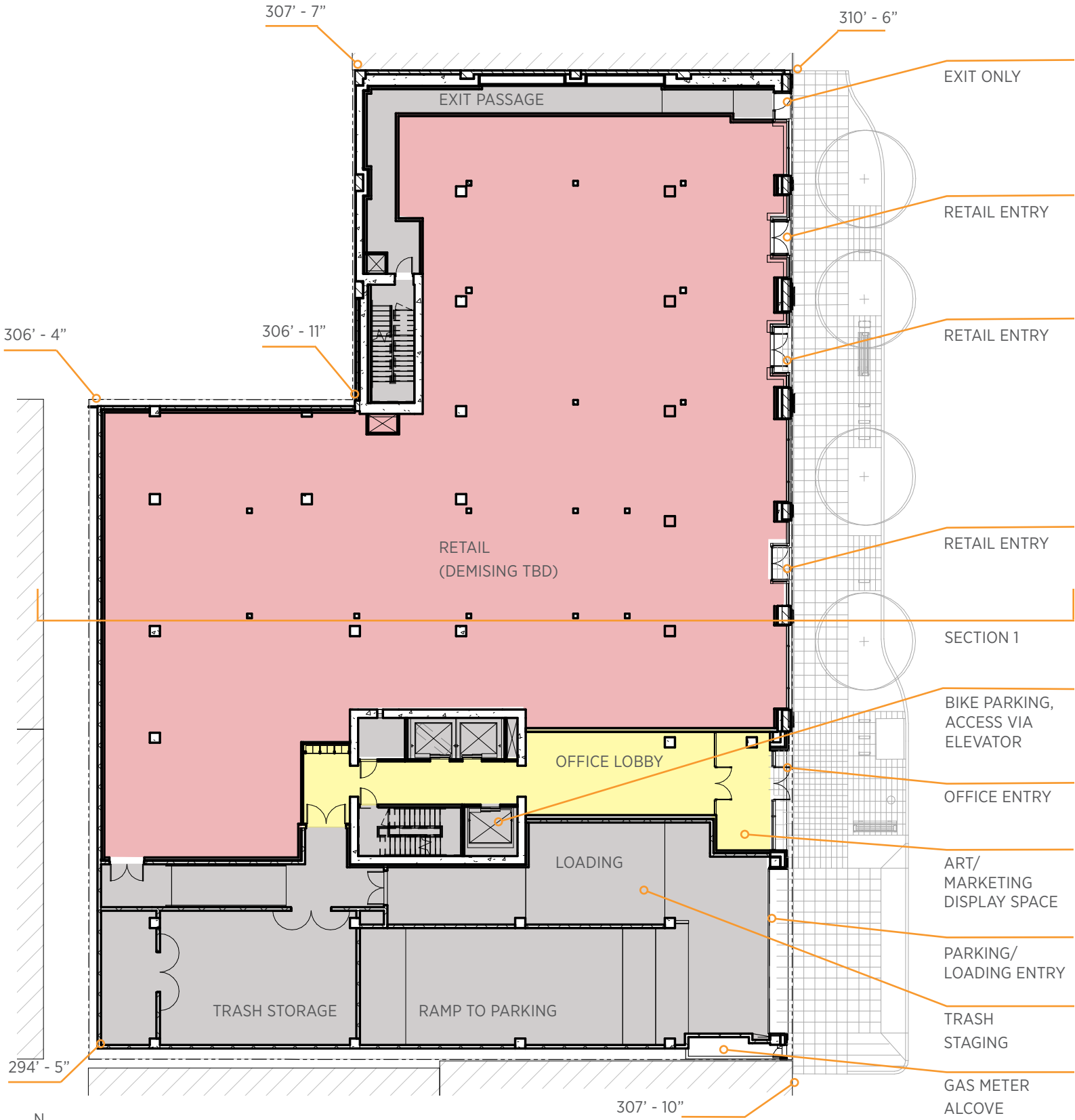
**PAGE INTENTIONALLY  
LEFT BLANK**



FLOOR PLANS

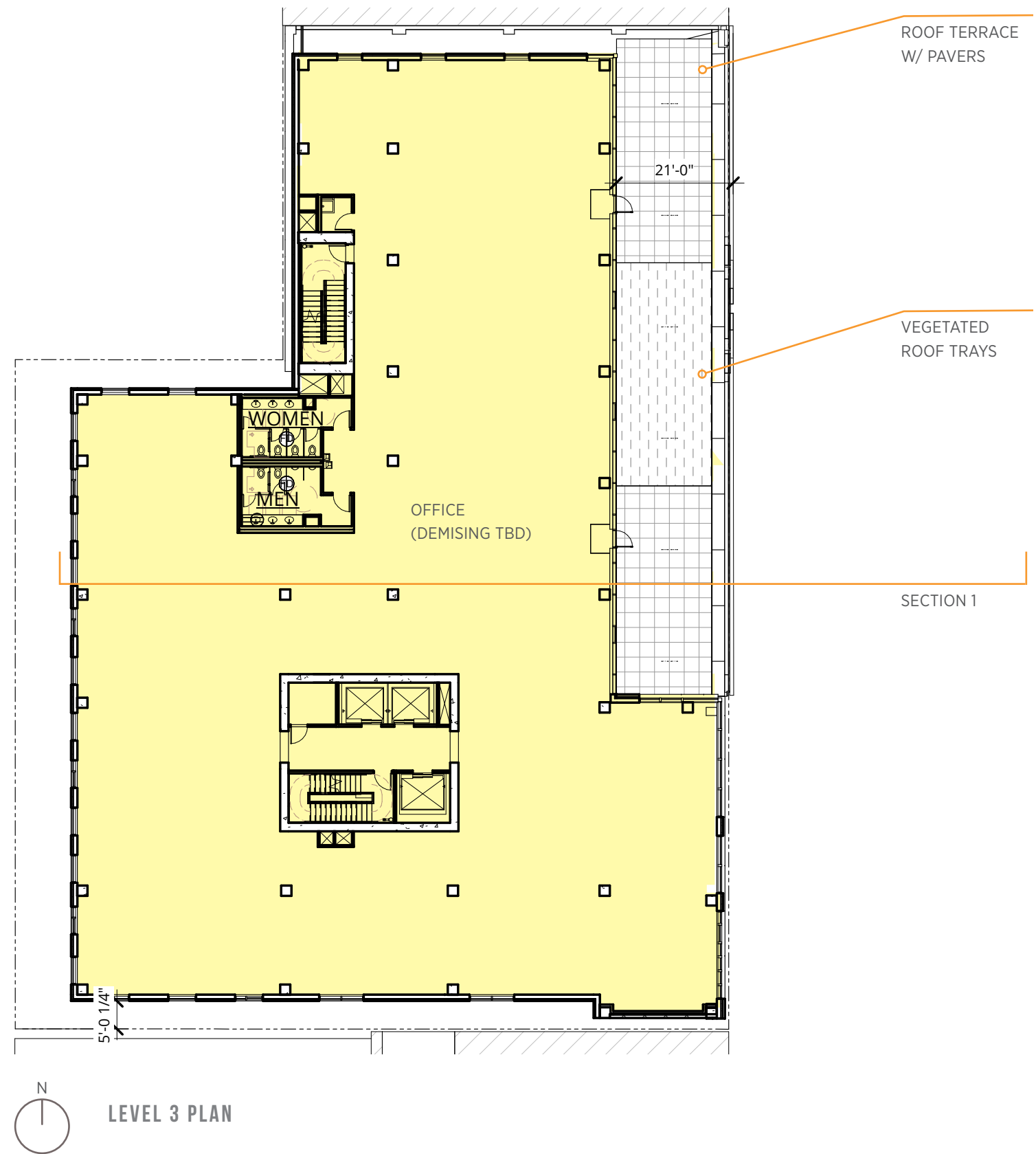
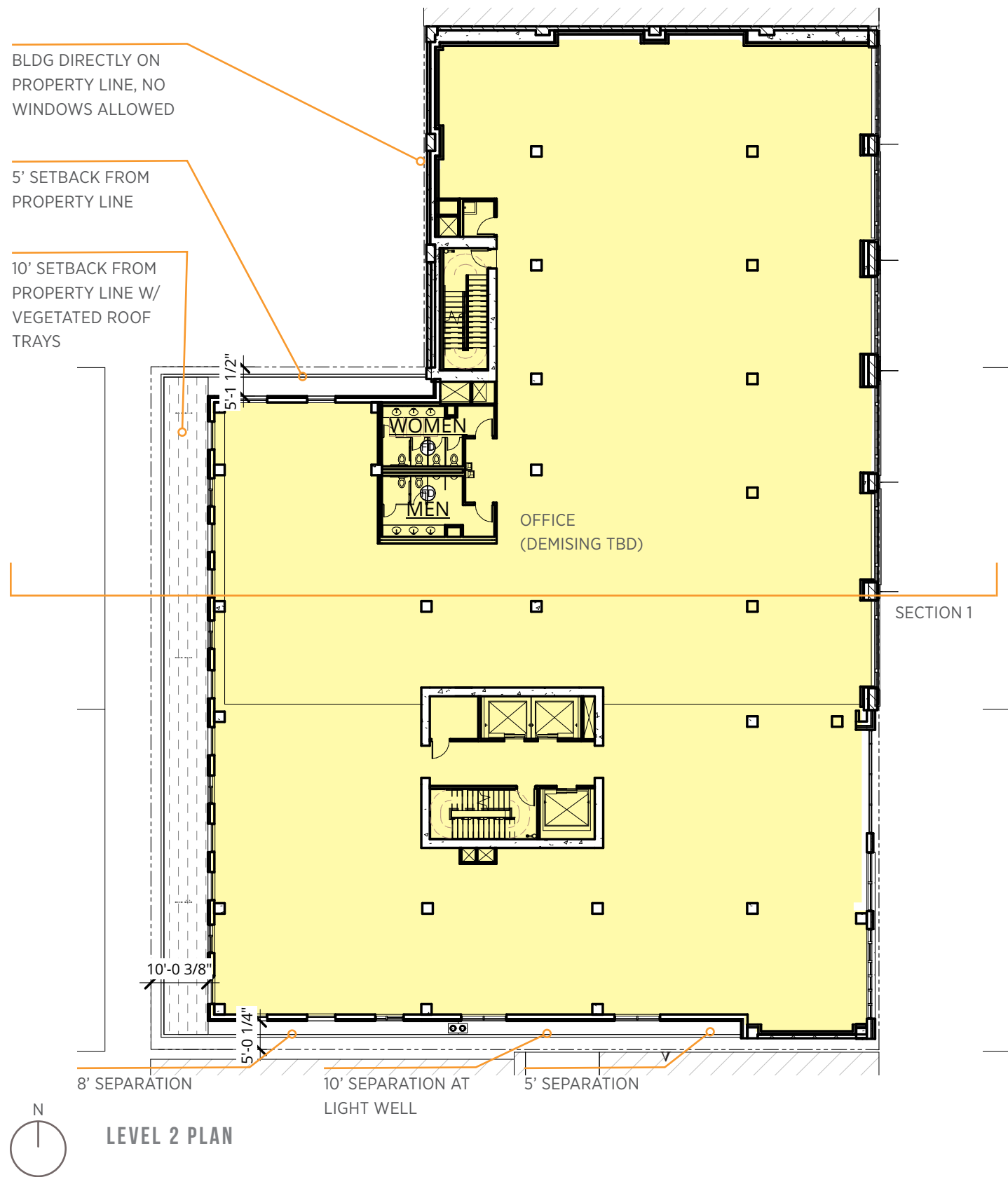


LEVEL P1 PLAN

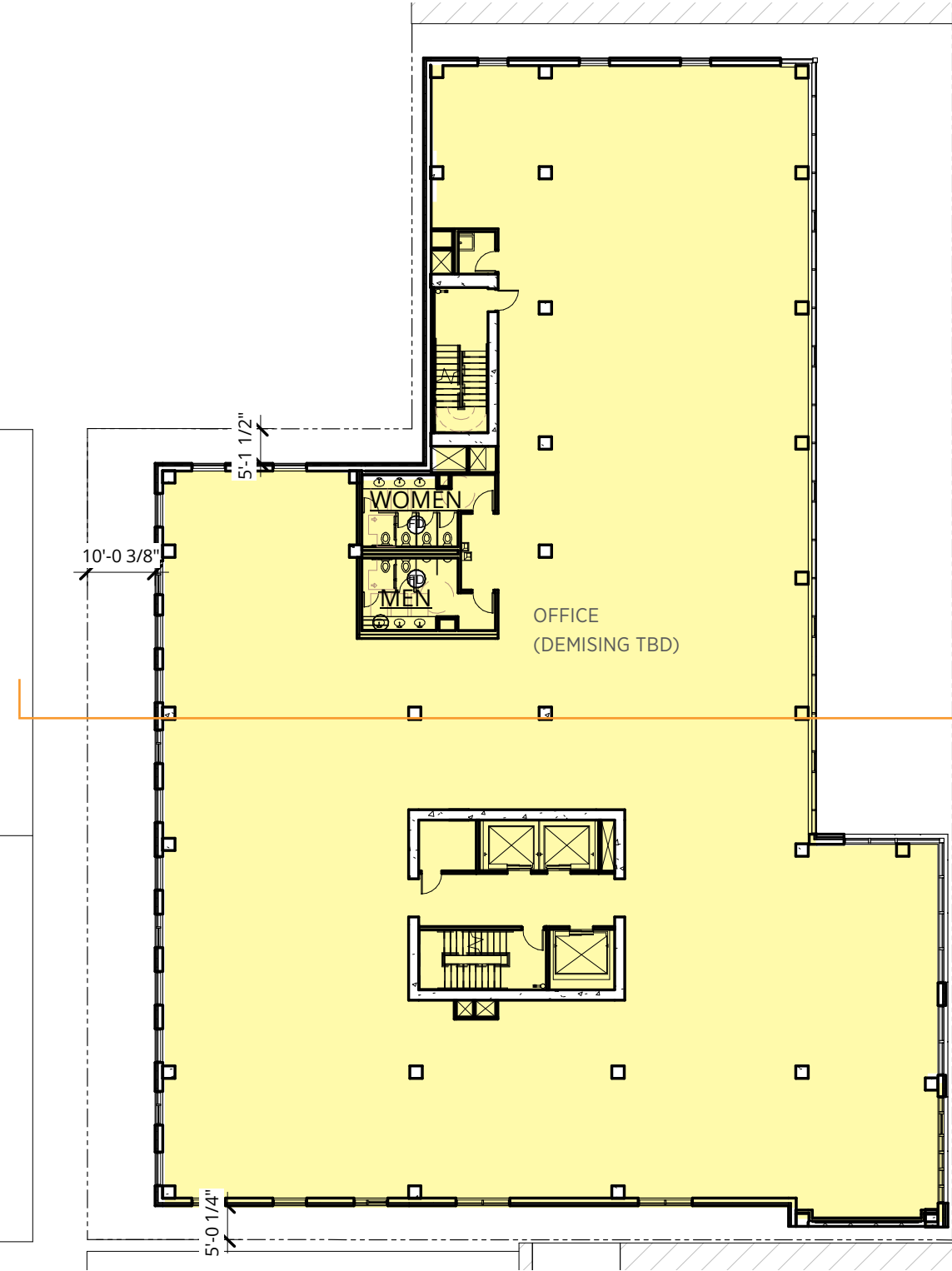


LEVEL 1 PLAN







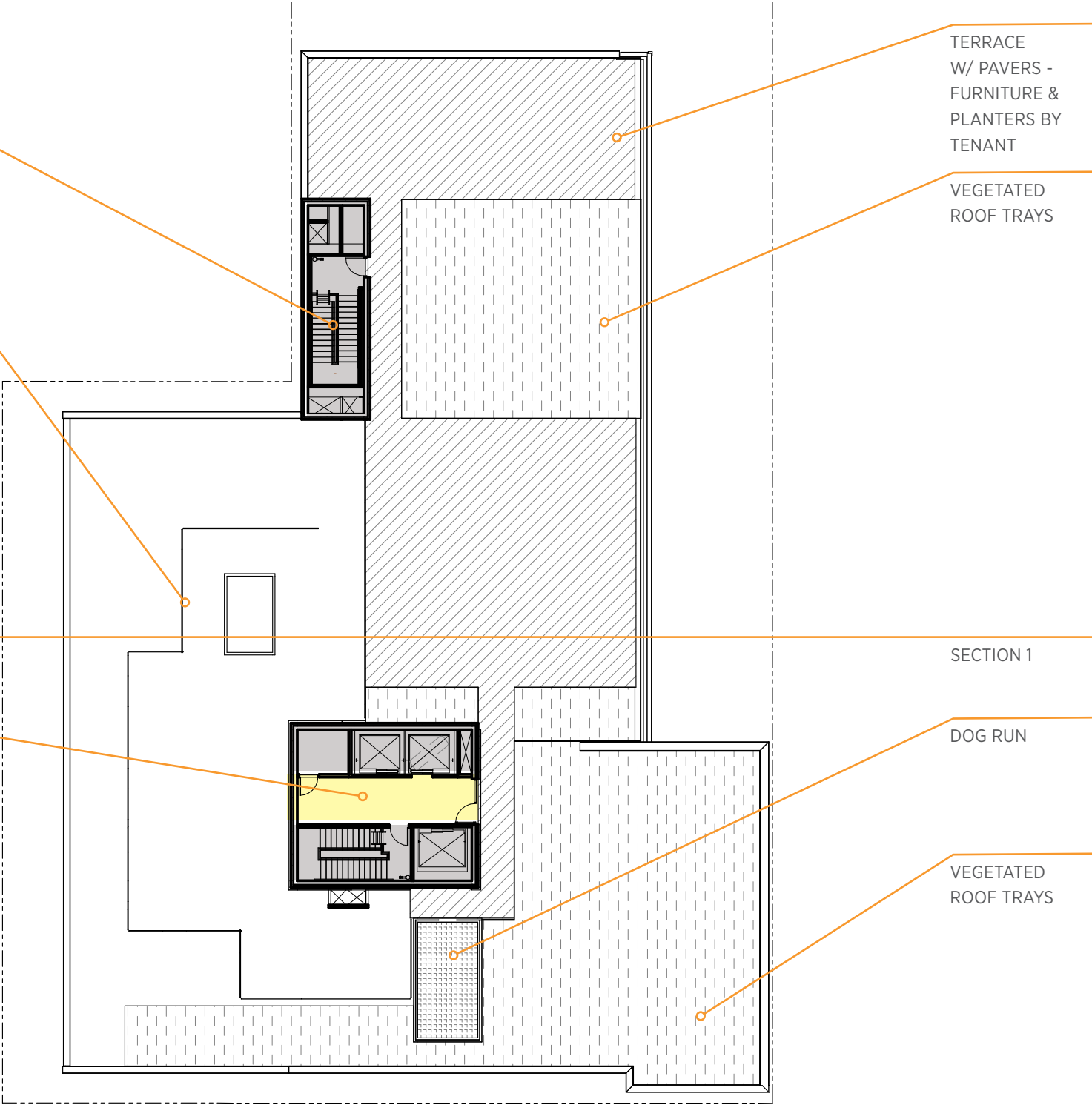


STAIR  
PENTHOUSE  
EXTENDS 16'  
ABOVE ROOF

SCREENED  
MECHANICAL  
EQUIPMENT  
EXTENDS -8'  
ABOVE ROOF

SECTION 1

STAIR/ELEV  
PENTHOUSE  
EXTENDS 16'  
ABOVE ROOF



TERRACE  
W/ PAVERS -  
FURNITURE &  
PLANTERS BY  
TENANT

VEGETATED  
ROOF TRAYS

SECTION 1

DOG RUN

VEGETATED  
ROOF TRAYS



LEVEL 4-5 PLAN



ROOF PLAN



**PAGE INTENTIONALLY  
LEFT BLANK**



LANDSCAPE PLAN



LAYERED PLANTINGS | STRUCTURED EVERGREEN JUXTAPOSED WITH FINE TEXTURE



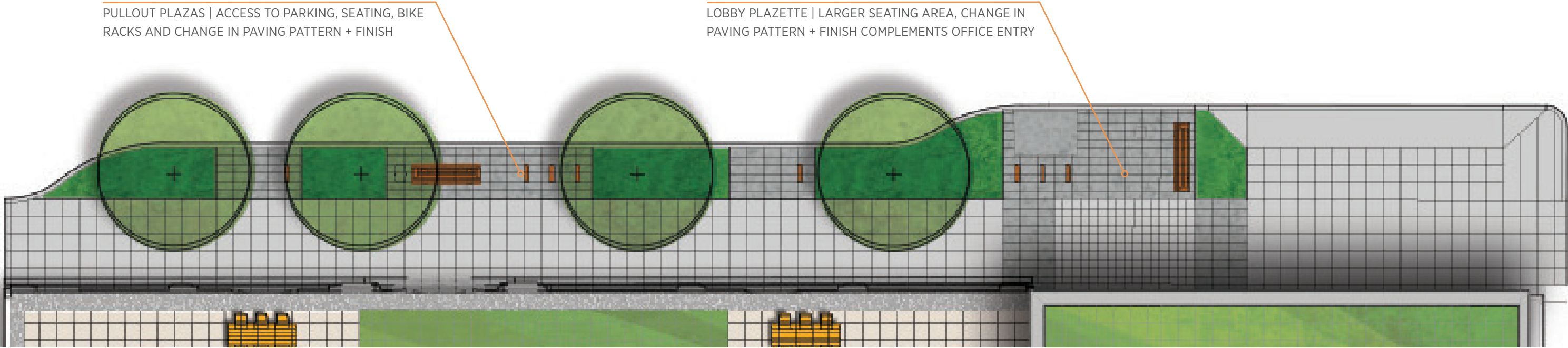
POLLINATOR PATHWAY | COLOR AND MOVEMENT



GROUNDCOVER BUFFER | EVERGREEN SURROUND



WILLOW OAK | LARGE CANOPY



STREET FURNITURE | DURABLE MATERIALS, BOLD COLOR, COMPLEMENTARY DESIGN

STREETSCAPE DESIGN

FOLLOWS CUES FROM 10TH AND 11TH STREET CONCEPT PLAN



ROOFTOP AND TERRACES

OUTDOOR SPACE WITH VIEWS OUT AND BACK TO STREET



FIFTH FACADE | ECO ROOF CREATES VISUAL INTEREST WITH A SEASONAL, TEXTURED 'RAINBOW CARPET' COMPRISED OF A VARIETY OF SEDUM SPECIES AT THREE LEVELS OF THE BUILDING



PHOTO: UW HOUSING BY GGN

ROOF DECK AND TERRACES | A SIMPLE PLATFORM PROVIDES OPPORTUNITES FOR FLEXIBLE SEATING AND FUTURE PERMANENT PLANTERS



PHOTO: WALL STREET JOURNAL

ROOFTOP DOG RUN | APPEAL TO ALL CROWDS. DOG RUN NESTLES INTO ECO ROOF SEPARATE FROM MAIN SEATING SPACE



COMPOSITE LANDSCAPE/HARDSCAPE PLAN



# MATERIAL AND COLOR PALETTE

## CEMENTITIOUS COMPOSITE PANEL FIBREC BY RIEDER

### TYPE 1

WHITE COLOR  
LIGHT SANDBLASTED TEXTURE



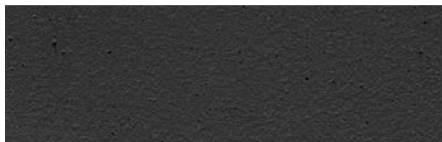
MFR FINISH CUTSHEET



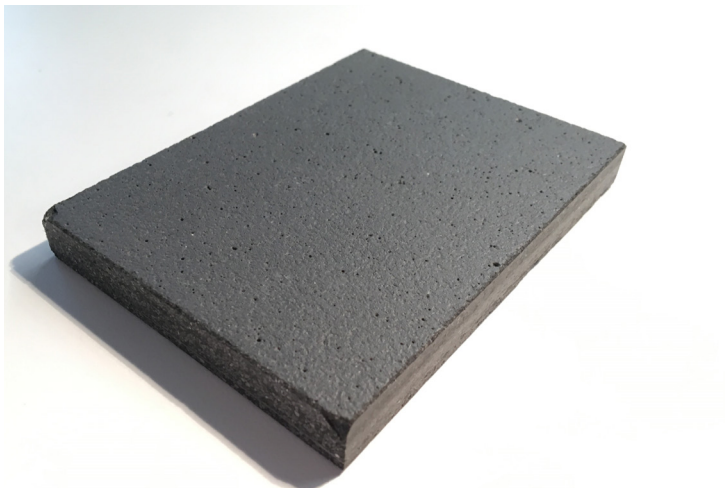
SAMPLE PHOTOGRAPH

### TYPE 2

DARK GREY COLOR  
LIGHT SANDBLASTED TEXTURE



MFR FINISH CUTSHEET

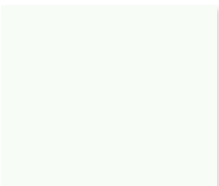


SAMPLE PHOTOGRAPH

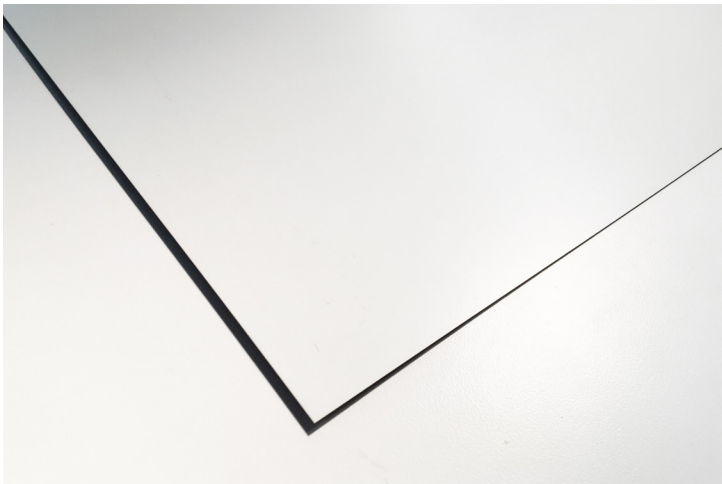
## METAL PANEL

### TYPE 1

WHITE COLOR



MFR FINISH  
CUTSHEET



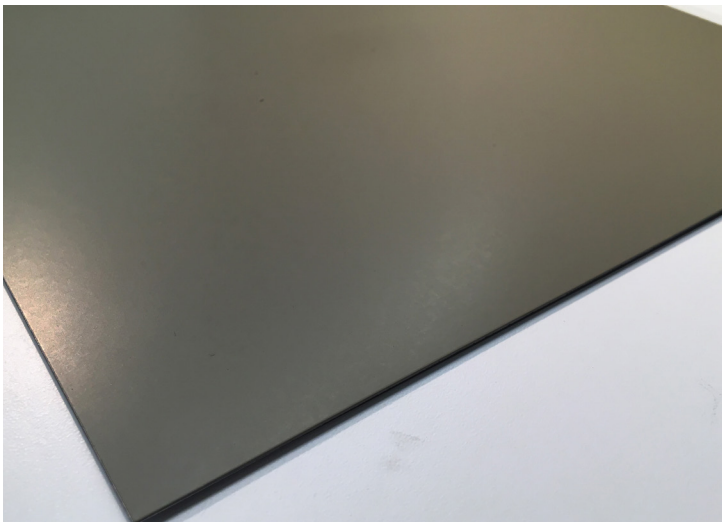
SAMPLE PHOTOGRAPH

### TYPE 2

DARK GREY COLOR

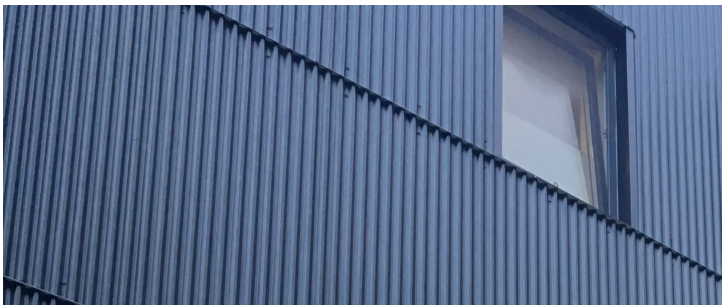


MFR FINISH  
CUTSHEET



SAMPLE PHOTOGRAPH

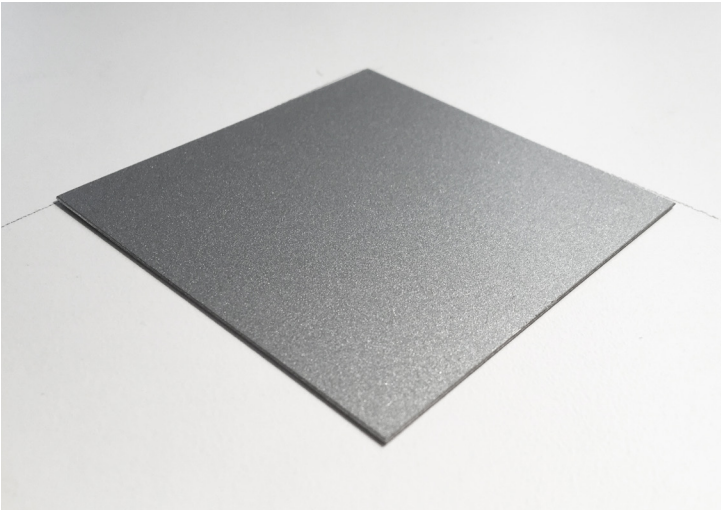
## CORRUGATION PATTERN & FASTENING EXAMPLE



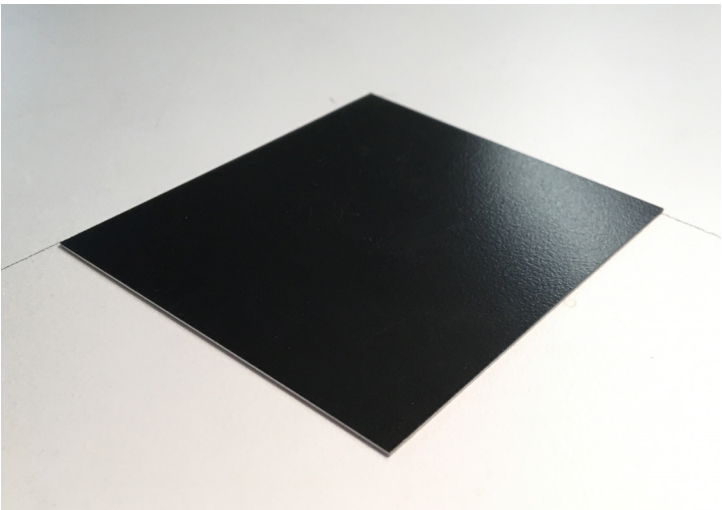


CURTAIN WALL / ALUMINUM WINDOWS

CURTAIN WALL TYPE 3  
ALUMINUM WINDOW TYPE 2  
SILVER MULLIONS/FRAMES



CURTAIN WALL TYPE 2  
ALUMINUM WINDOW TYPE 1  
BLACK MULLIONS/FRAMES

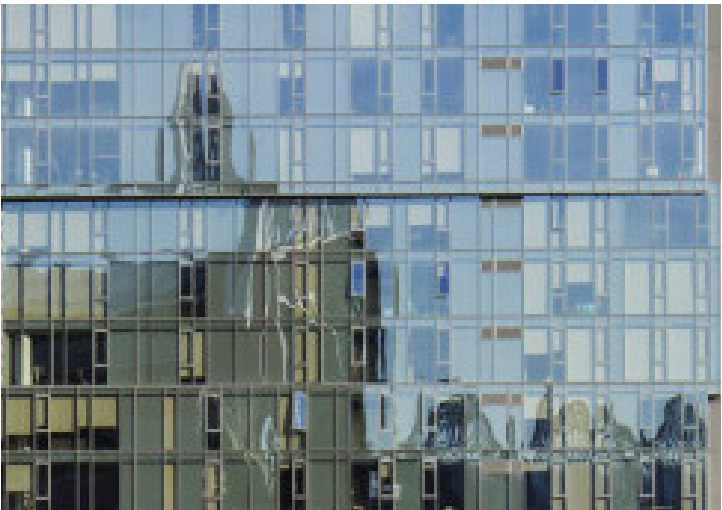


CURTAIN WALL TYPE 1  
WOOD WINDOWS  
FINISH COLOR TO MATCH EXISTING WOOD  
WINDOWS AND STOREFRONT

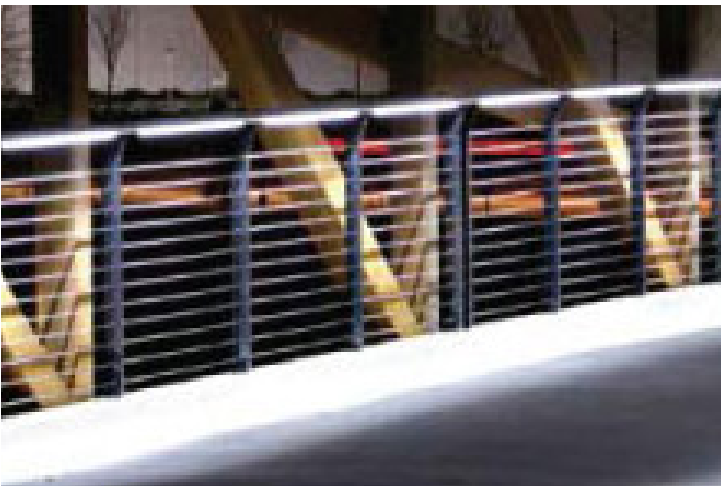


MISCELLANEOUS

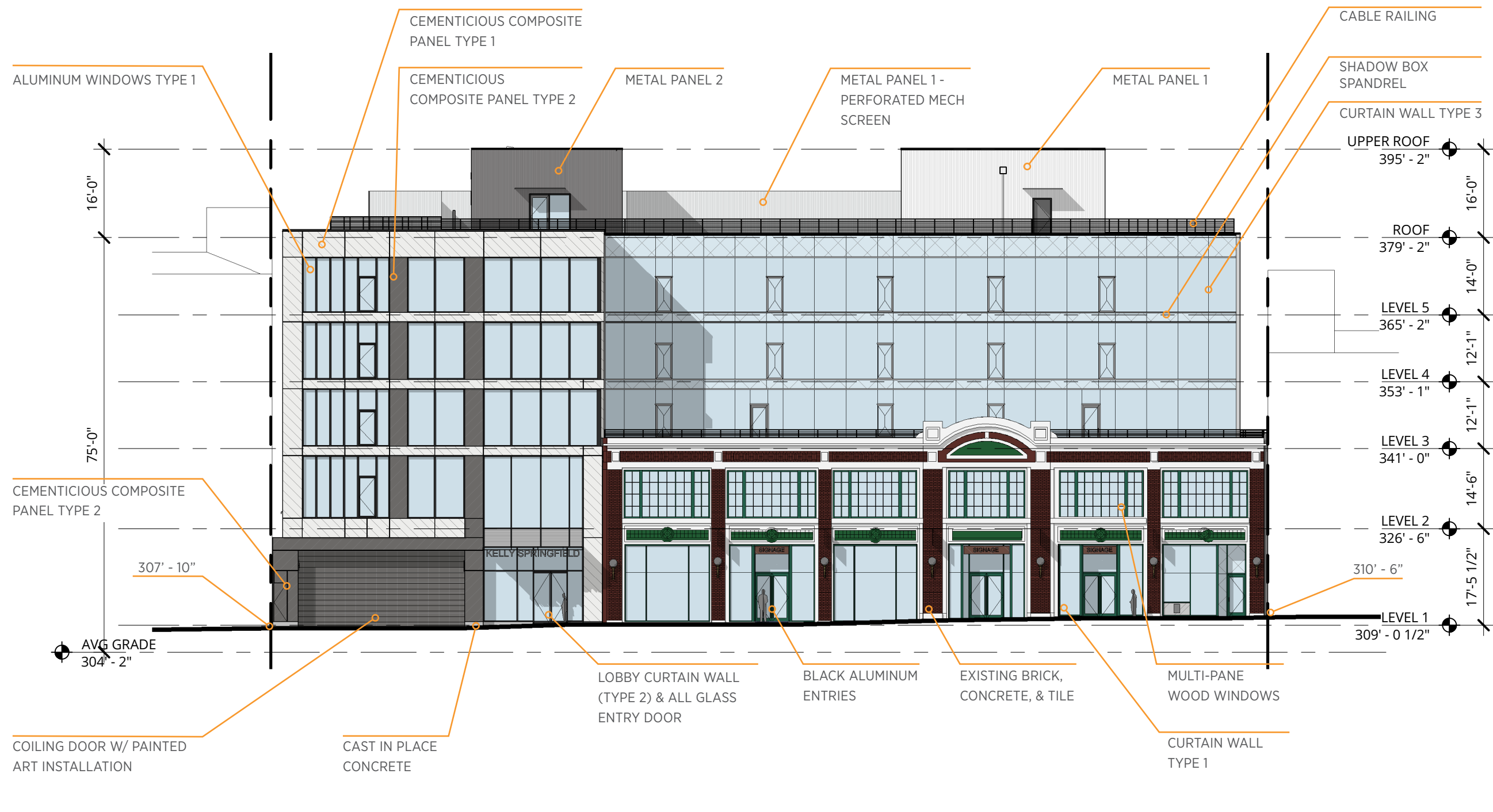
SHADOW BOX SPANDREL PANELS



CABLE RAILING W/ LIGHTED HANDRAIL

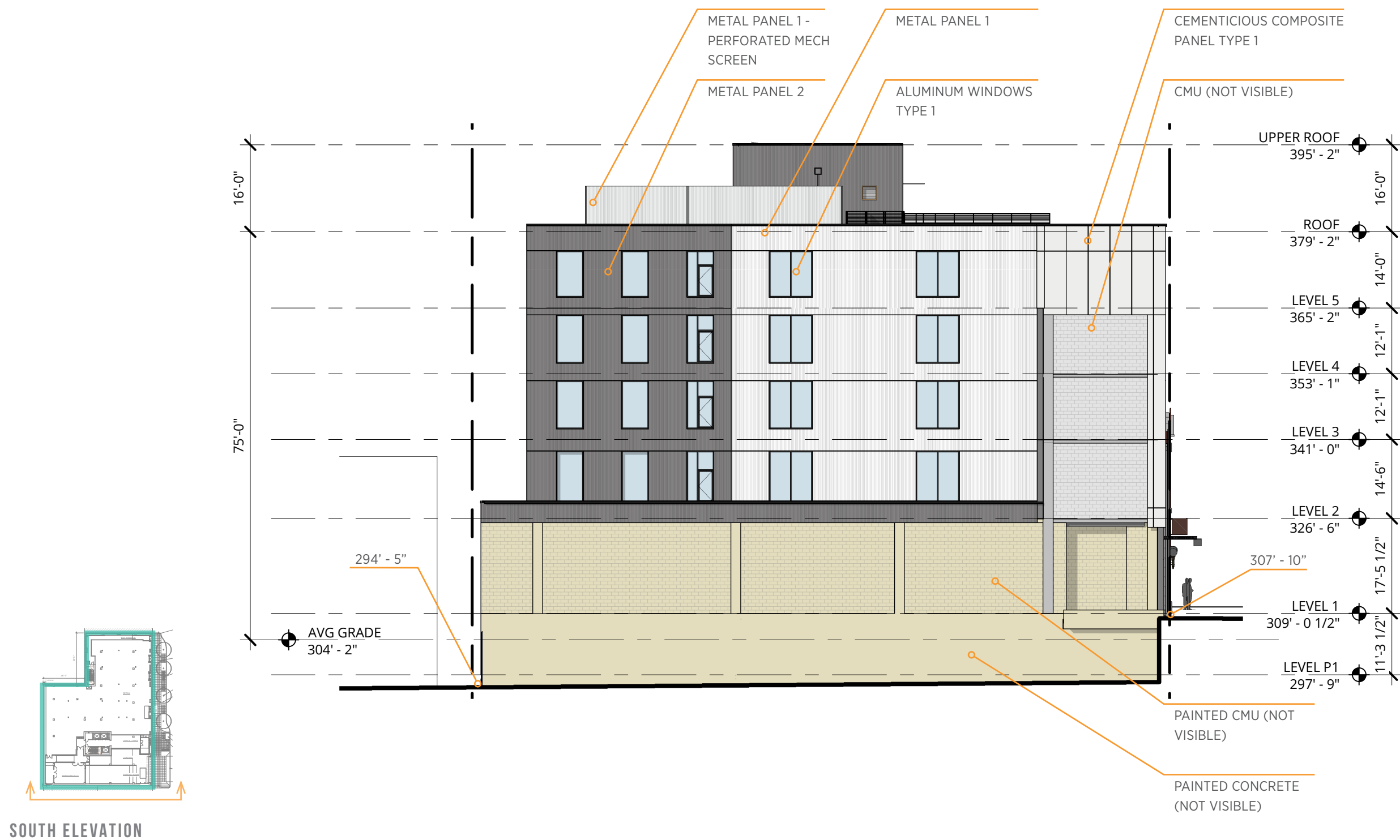




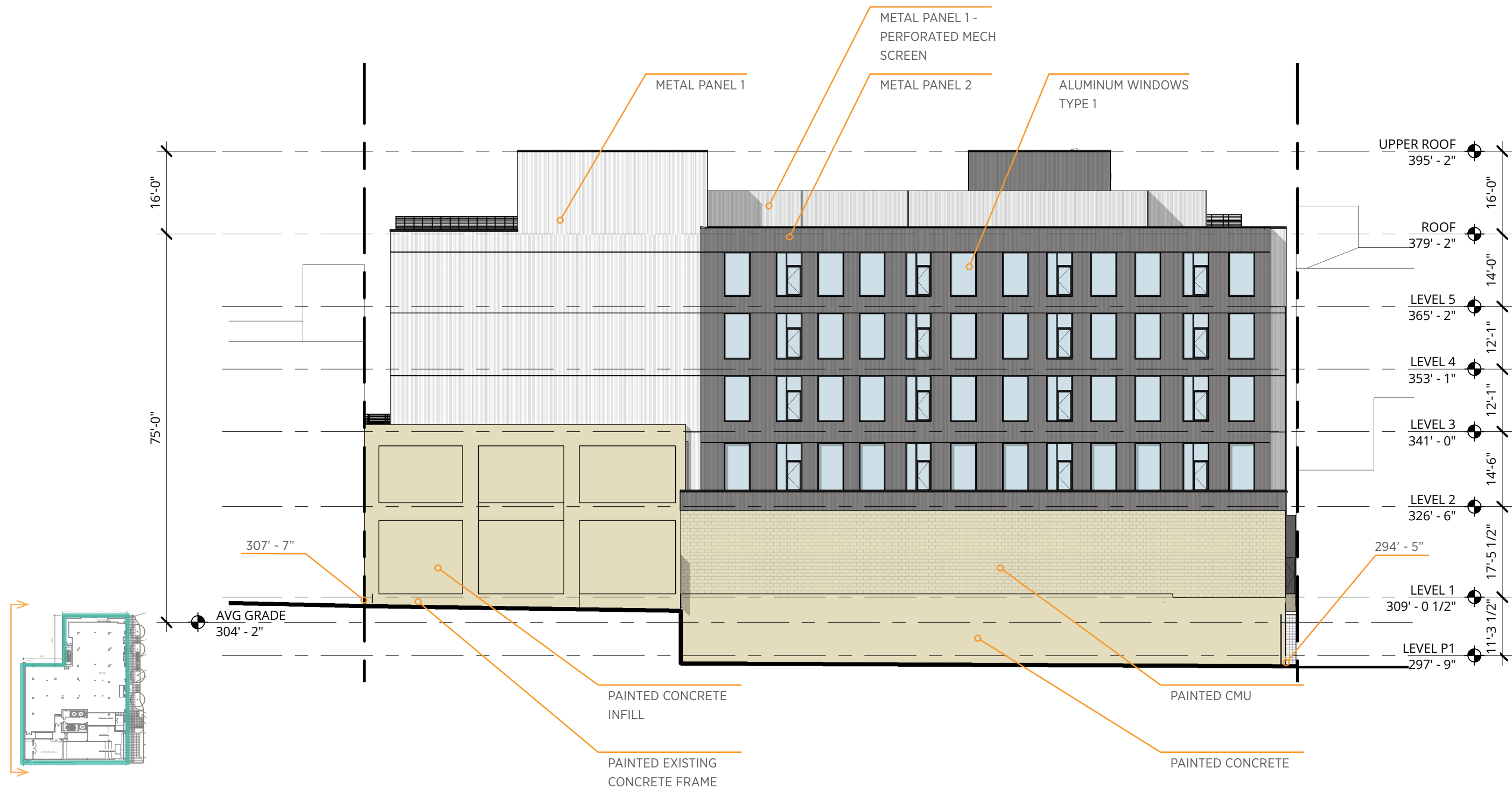


EAST ELEVATION









WEST ELEVATION

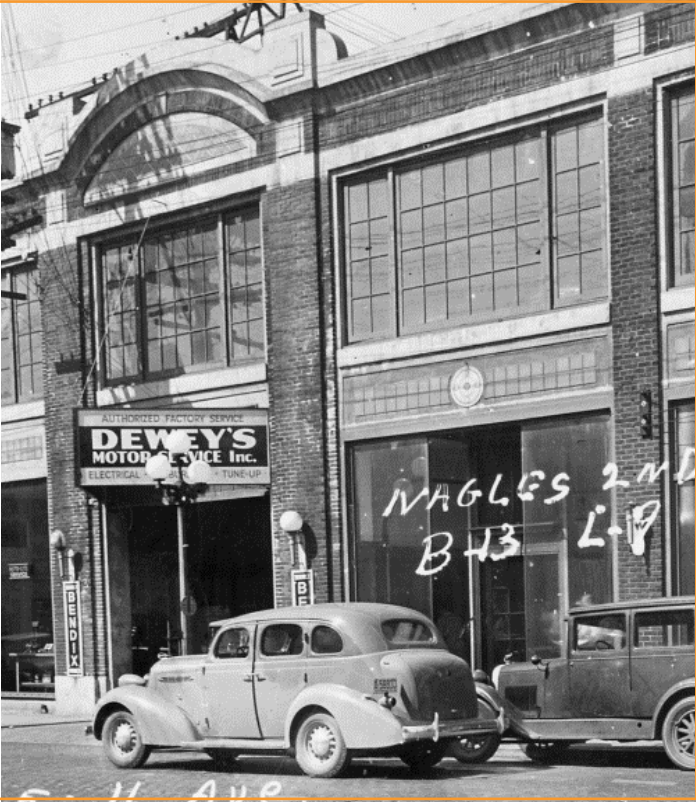
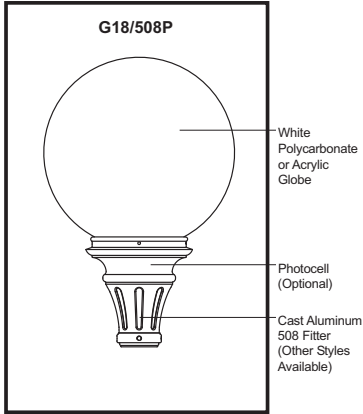
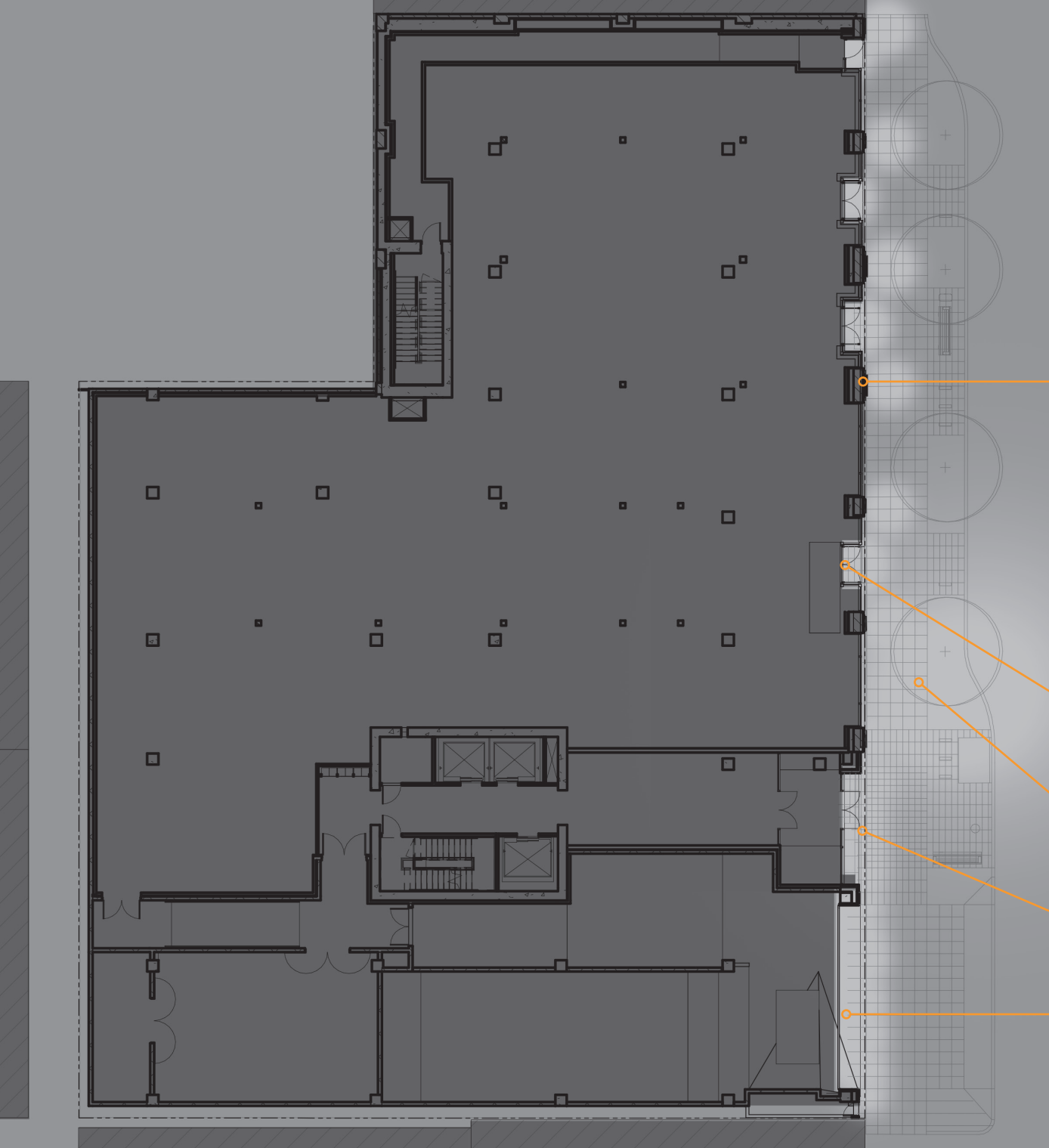




NORTH ELEVATION



EXTERIOR LIGHTING PLAN



SLIM SURFACE  
MOUNTED DOWNLIGHTS  
IN SOFFIT, TYPICAL



CITY OF SEATTLE  
STREET LIGHT

LINEAR FIXTURES  
MOUNTED ON ENTRY  
CANOPY

SMALL "PUCK" STYLE  
LIGHTS MOUNTED IN  
SOFFIT ABOVE GARAGE  
ENTRANCE

FIXTURE TO BE BLACK

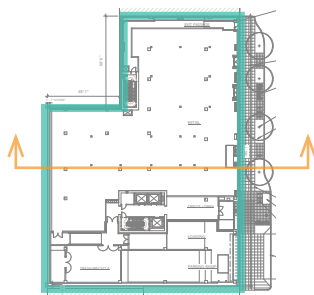
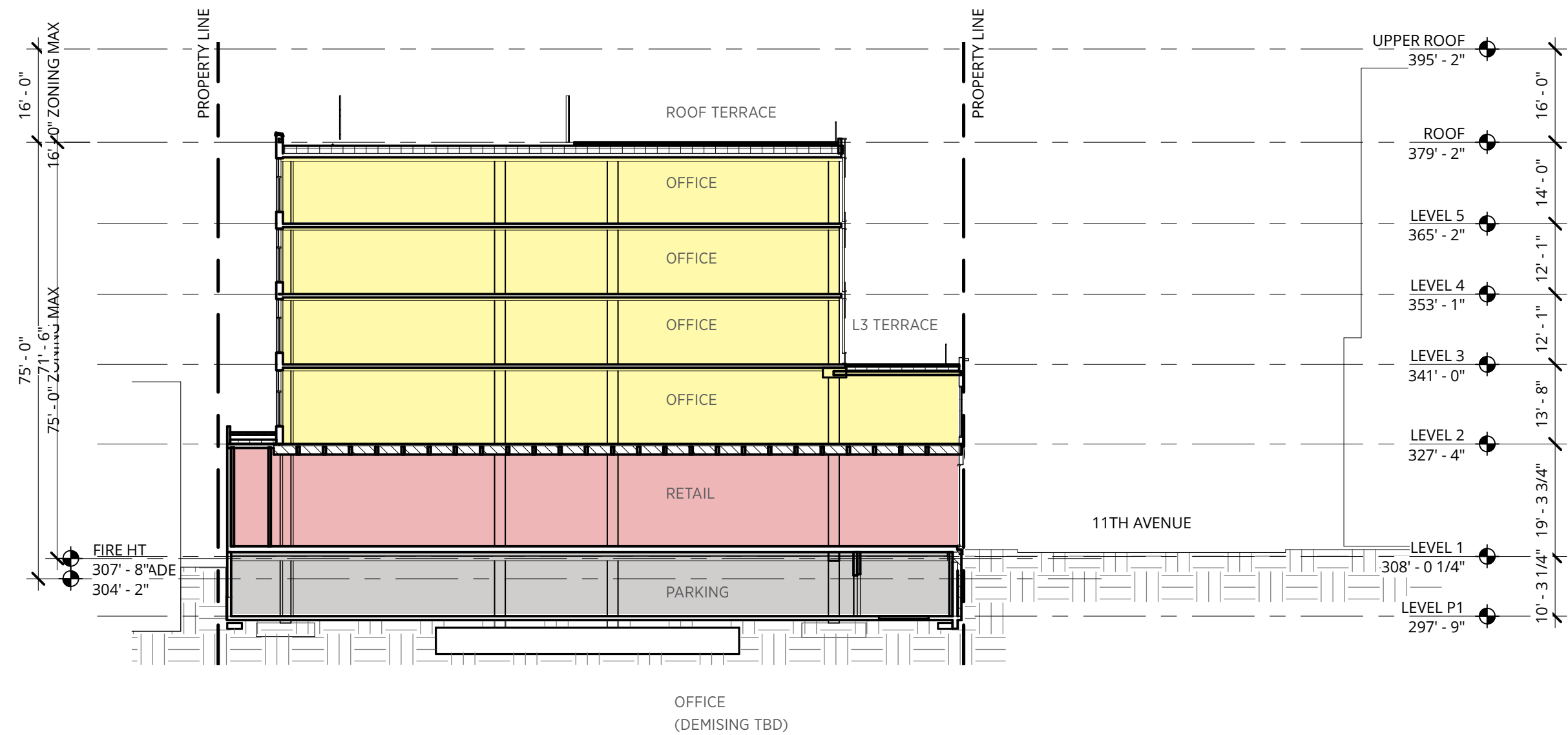


SITE LIGHTING PLAN



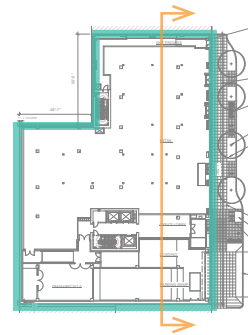
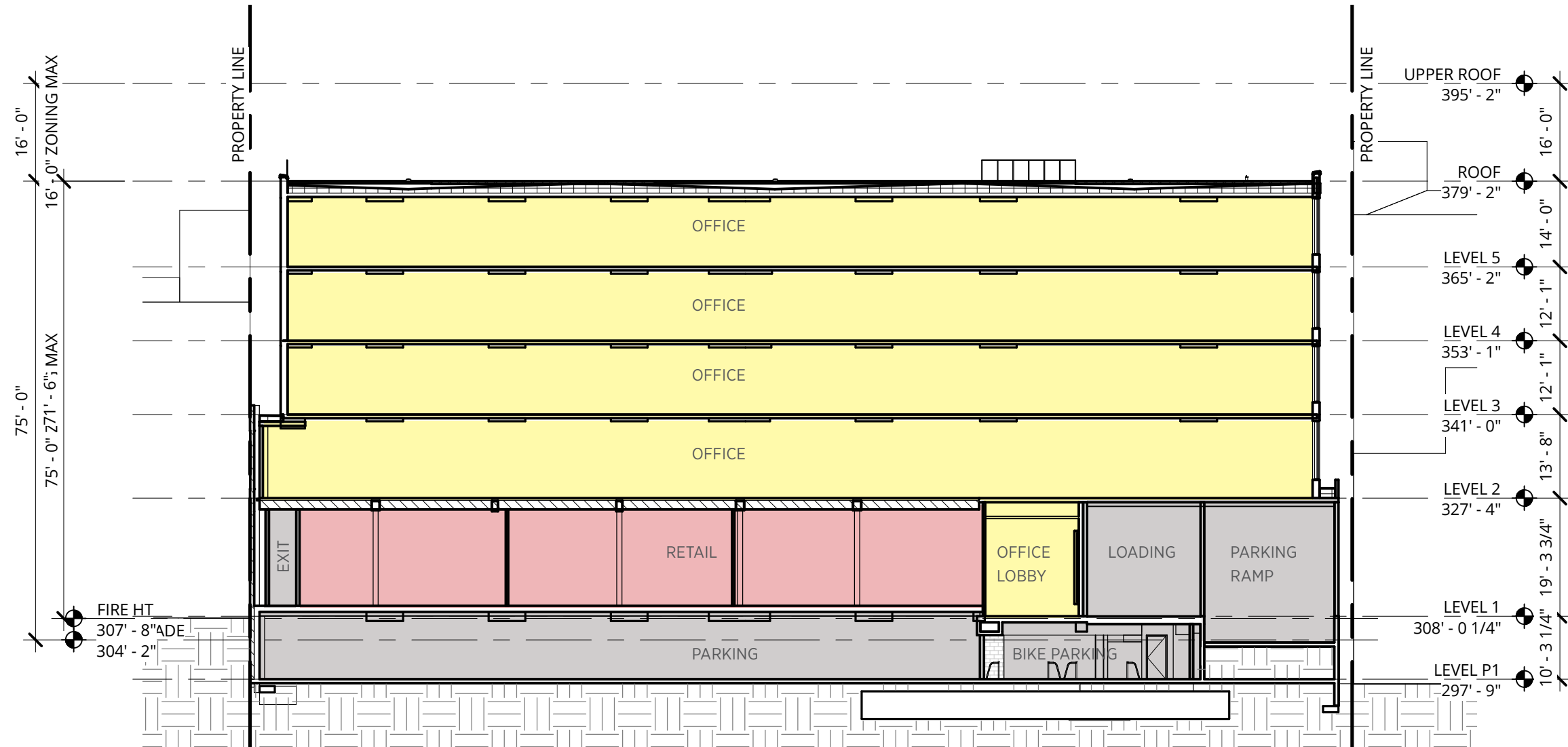
**PAGE INTENTIONALLY  
LEFT BLANK**





EAST/WEST BUILDING SECTION





NORTH/SOUTH BUILDING SECTION



EAST ELEVATION RENDERING





EAST ELEVATION RENDERING (NIGHT RENDERING)





11TH AVE PERSPECTIVE LOOKING SOUTHWEST





CURTAIN WALL CLOSEUP





RETAIL PERSPECTIVE LOOKING SOUTH ALONG 11TH AVENUE





OFFICE LOBBY PERSPECTIVE LOOKING NORTH UP 11TH AVENUE





PERSPECTIVE OF EAST ELEVATION LOOKING NORTHWEST





AERIAL PERSPECTIVE OF EAST ELEVATION LOOKING NORTHWEST





PERSPECTIVE OF NORTH AND WEST ELEVATIONS





PHOTOGRAPH OF EXISTING CONDITION FOR COMPARISON





ANTICIPATED DEPARTURES

Code Section	Departure Request	Rationale for Departure Request
<b>23.54.035.C.1</b> Loading Berth Width and Clearance	<p>Loading berths are required to have a vertical clearance of 14’</p> <p>The project intends for two of the required loading berths to be located in the below grade garage. These berths will not achieve a 14’ vertical clearance.</p>	<p>Locating additional loading berths on 11th Avenue would strongly degrade the streetscape and conflict with many design guidelines. To avoid this, the project accommodates part of the required loading below grade. Providing a 14’ vertical clearance to these berths would require an extreme amount of excavation. A 14’ vertical clearance is not necessary to accommodate the types of loading that will occur in these berths.</p>
<b>23.73.014.B.3</b> Additional Height Above a Character Structure	<p>A departure is required to use the 10’ of additional height allowed above a character structure within the Pike/Pine Overlay for a nonresidential use.</p> <p>The project proposes to use the extra 10’ for office space.</p> <p>The zoning code states that the decision to allow a departure should be based on a judgment that the project has achieved one of several goals, including maintaining greater portions of existing character structures through design treatments that exceed the minimum standards of subsection 23.73.015.A.</p>	<p>The proposed project does not only preserve the street facing facade and ground level floor to ceiling height as required in subsection 23.73.015.A, but also preserves portions of the original side and rear walls and substantial interior elements.</p> <p>In addition to satisfying the stated criteria for this departure, the preservation of historic elements beyond what is required by zoning will allow the project to respond strongly to CS3.B.2. Historical /Cultural References and DC2.II. Integration of Character Structures.</p>
<b>23.54.030.G.2</b> Sight Triangles	<p>Sight triangles are required on the exit side for driveways more than 22 feet wide.</p> <p>The project proposes to use mirrors and/or visible and audible alarms to maintain pedestrian safety in lieu of sight triangles.</p>	<p>Meeting this requirement would substantially widen the garage entries on 11th Avenue, conflicting with goals to create a pedestrian friendly streetscape, conflicting with DC1.I. Location of Parking and PL3.II. Human Scale.</p>



LOADING BERTH WIDTH AND CLEARANCE

ZONING REQUIREMENTS

Loading berths are required to have a vertical clearance of 14 feet. (SMC 23.54.035.C.1.)

The project proposes to place two of the three required loading berths in the below grade garage. These berths will have a vertical clearance of less than 14 feet.

The minimum clear height on the path to the loading berths in the garage is 8' - 2", the minimum clearance for ADA vans. This height will allow most vans and pickup style trucks to use the designated loading stalls.

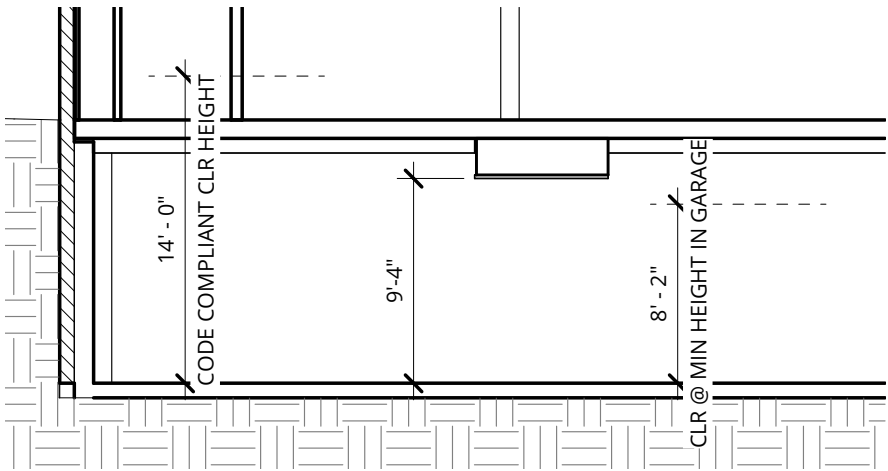
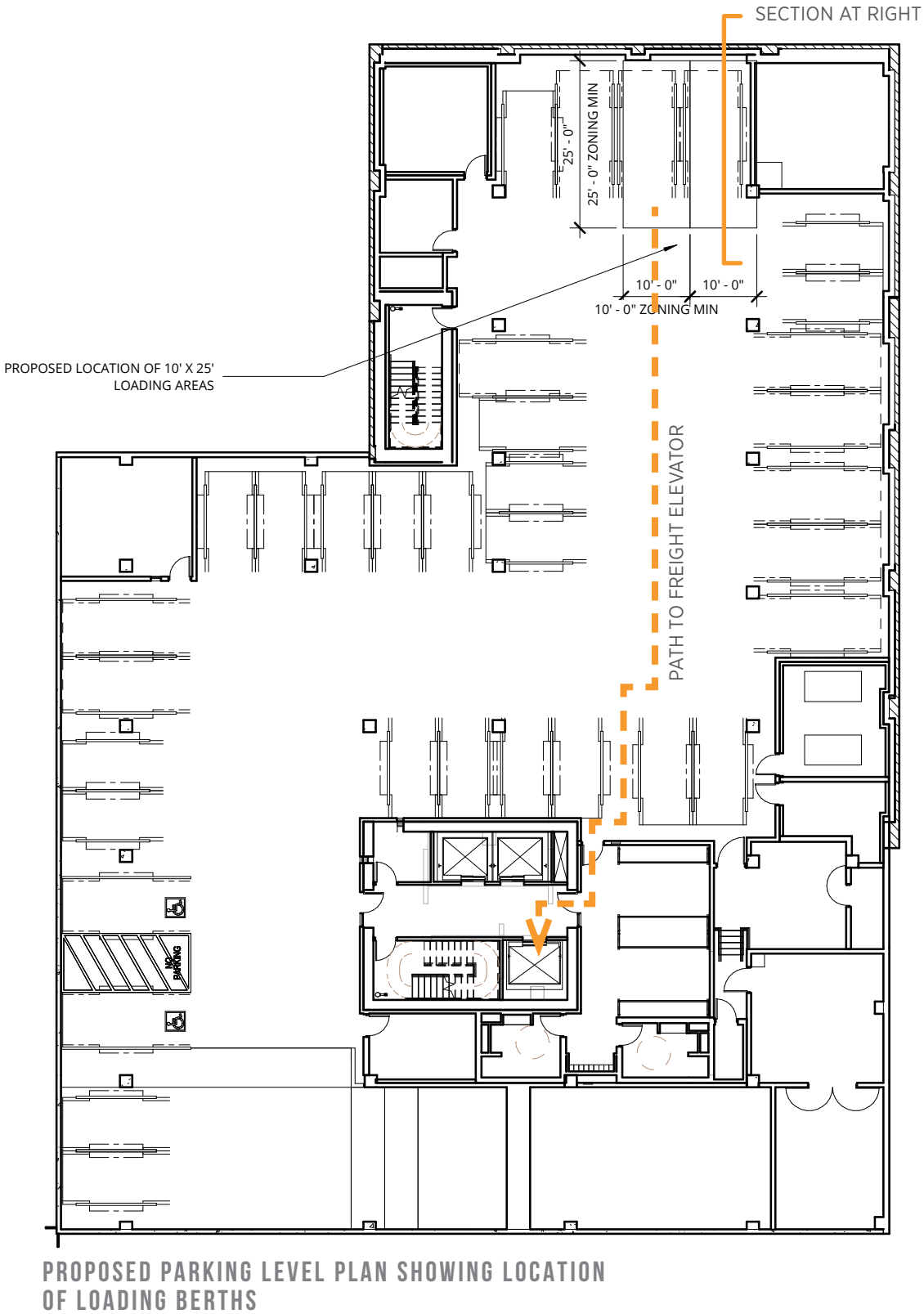
WHY THIS DEPARTURE BETTER MEETS DESIGN GUIDELINES

Locating all required loading berths on 11th avenue would strongly degrade the streetscape and reduce pedestrian friendliness, conflicting with design guidelines PL1-B-2 Pedstrian Volumes, DC1-B-1 Access Location and Design, DC1-C-4 Service Uses, and DC1-C-1 Below-Grade Parking.

To avoid these negative impacts, the project proposes to accomodate two of the three required loading berths below grade. Providing a 14 foot vertical clearance to these berths would require an unnecessary increase in the quantity of excavation and would increase the length of the garage entry ramp, reducing usable square footage. A 14 foot vertical clearance is not necessary to accomodate the types of loading that will occur in these berths.

Note that the loading configuration presented at DRB has been reviewed by city transportation staff, who have concluded that the proposal is acceptable.

Loading berths in the below grade garage are located where the largest possible clearances are available.





ADDITIONAL HEIGHT ABOVE A CHARACTER STRUCTURE

ZONING REQUIREMENTS

A departure is required to use the 10’ of additional height allowed above a character structure within the Pike/Pine Overlay for a nonresidential use.

The zoning code states that the decision to allow a departure should be based on a judgment that the project has achieved one of several alternate goals, including maintaining greater portions of existing character structures through design treatments that exceed the minimum standards of subsection 23.73.015.A.

(SMC 23.73.014.B.3.)

The project proposes to use the addition 10’ of height allowed under the Pike/Pine overlay for nonresidential use.

WHY THIS DEPARTURE BETTER MEETS DESIGN GUIDELINES

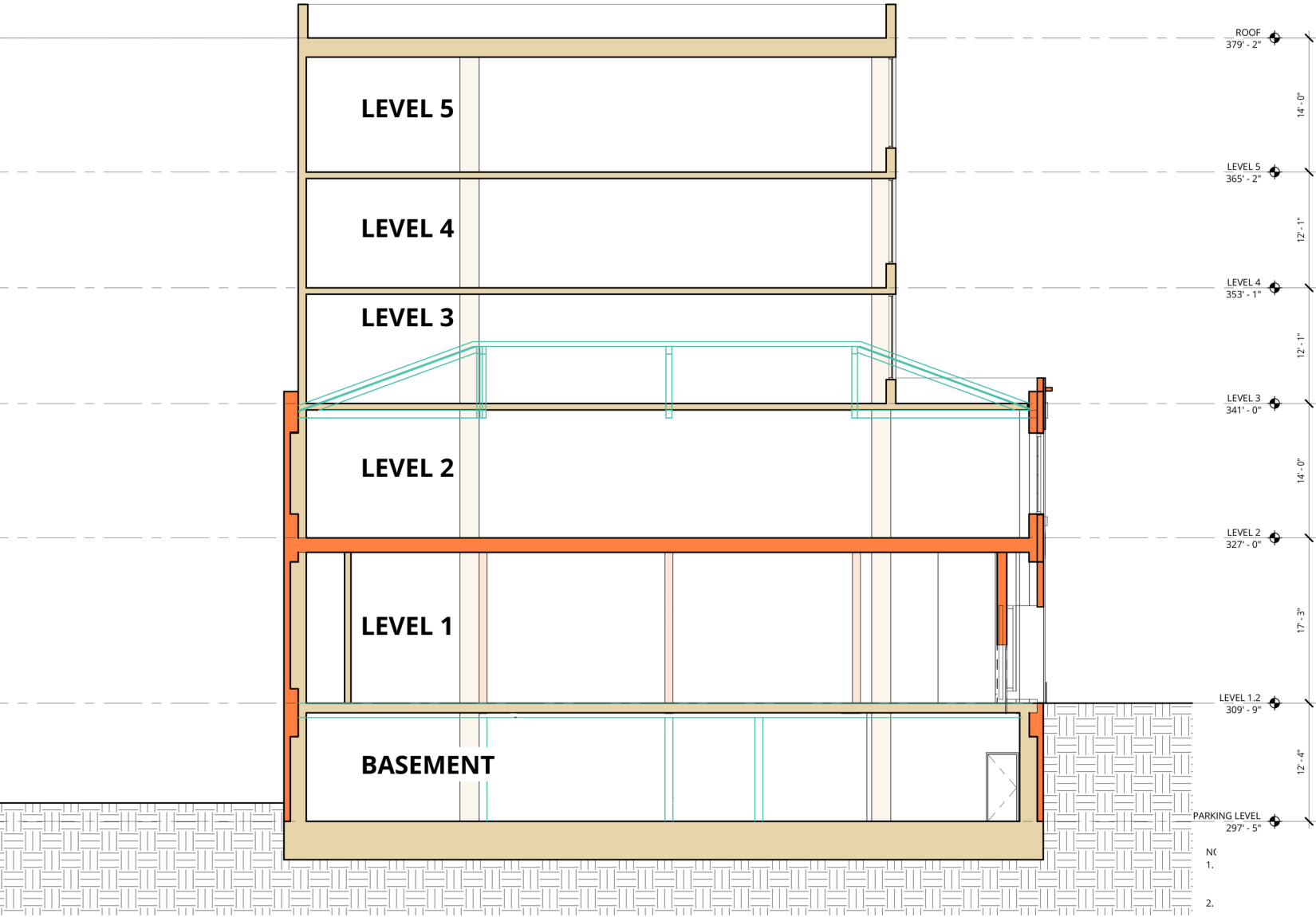
The minimum standards of 23.73.015A are as follows:

- All street-facing facades must be maintained
- New structures over character structure be set 15’ back from the facade of the character structure.
- The original floor to ceiling height of the street-level story of the character structure must be maintained except for accessibility accommodations.

The proposed project does not only comply with the above requirements but also preserves portions of the original side and rear walls and substantial interior elements.

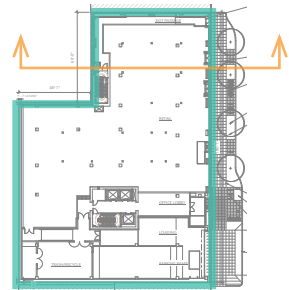
In addition to satisfying the stated criteria for this departure, the preservation of these elements beyond what is required by zoning will allow the project to respond strongly to CS3..B.2 Historical / Cultural References and DC2.II. Integration of Character Structures.

Note that neighborhood groups such as the Pike Pine Urban Neighborhood Council have expressed support for additional office space in the neighborhood, counterbalancing the strong trend towards residential development in recent years.



EAST/WEST SECTION

1/8" = 1'-0"



- EXISTING ELEMENT TO REMAIN
- EXISTING ELEMENT TO BE REMOVED
- PROPOSED NEW ELEMENT



IMAGERY OF INTERIOR WOOD FRAMING TO BE RETAINED





SIGHT TRIANGLES

ZONING REQUIREMENTS

For two-way driveways or easements less than 22’ wide, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10’ from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk. (SMC 23.54.030)

The proposed design does not include compliant site triangles.

WHY THIS DEPARTURE BETTER MEETS DESIGN GUIDELINES

11th Avenue is a principal pedestrian street and is focused on providing a pedestrian-friendly environment. The proposed design puts the street level pedestrian development standards and character structure response first.

Introducing a required sight triangle would increase the total width of the auto dominated portion of the east facing facade by 10’, increasing the impact of automobiles on the pedestrian environment, conflicting with DC1.I. Location of Parking. This would also reduce street transparency, conflicting with DC2.I. Height, Bulk, and Scale Compatibility and Proportions.

Sight triangles are not always implemented, as demonstrated by the images of other projects on this page which have not provided sight triangles.

The project will use mirrors to maintain pedestrian safety in lieu of sight triangles.

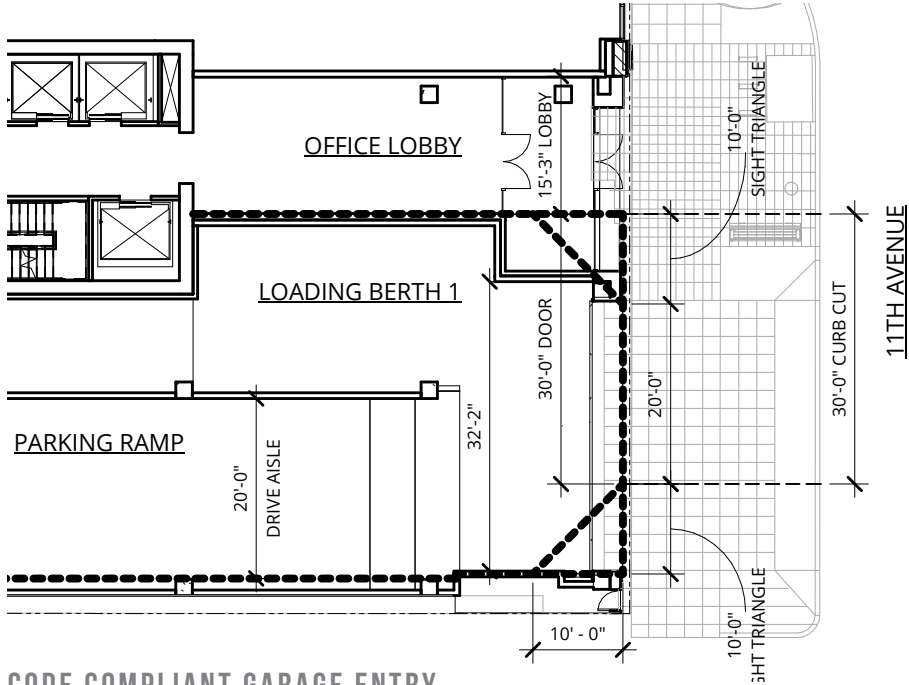
NEIGHBORHOOD PRECEDENTS



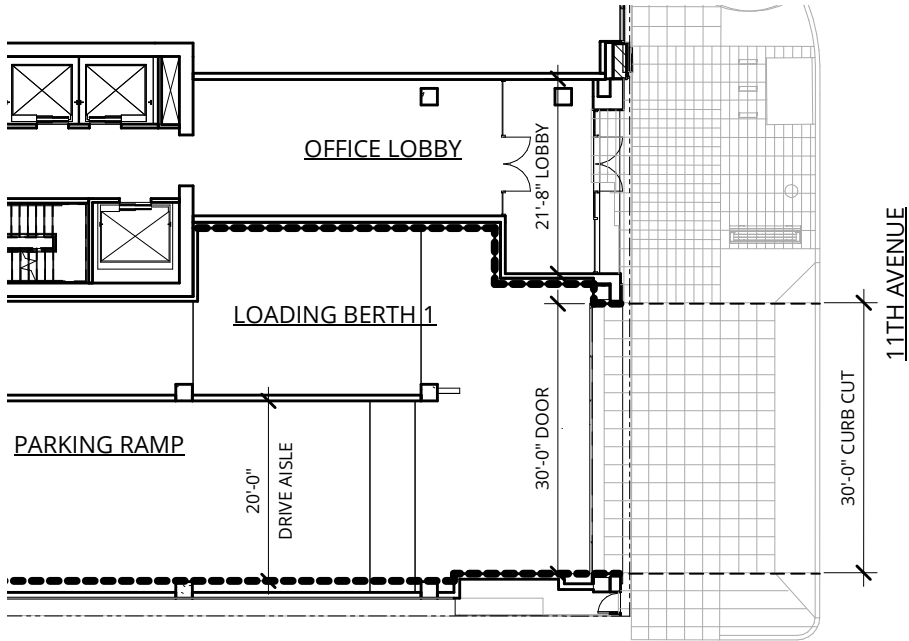
TRACE NORTH  
NO EXIT SIDE SIGHT TRIANGLE OR MIRRORS



SEATTLE POLICE DEPARTMENT  
USE OF MIRRORS



CODE COMPLIANT GARAGE ENTRY



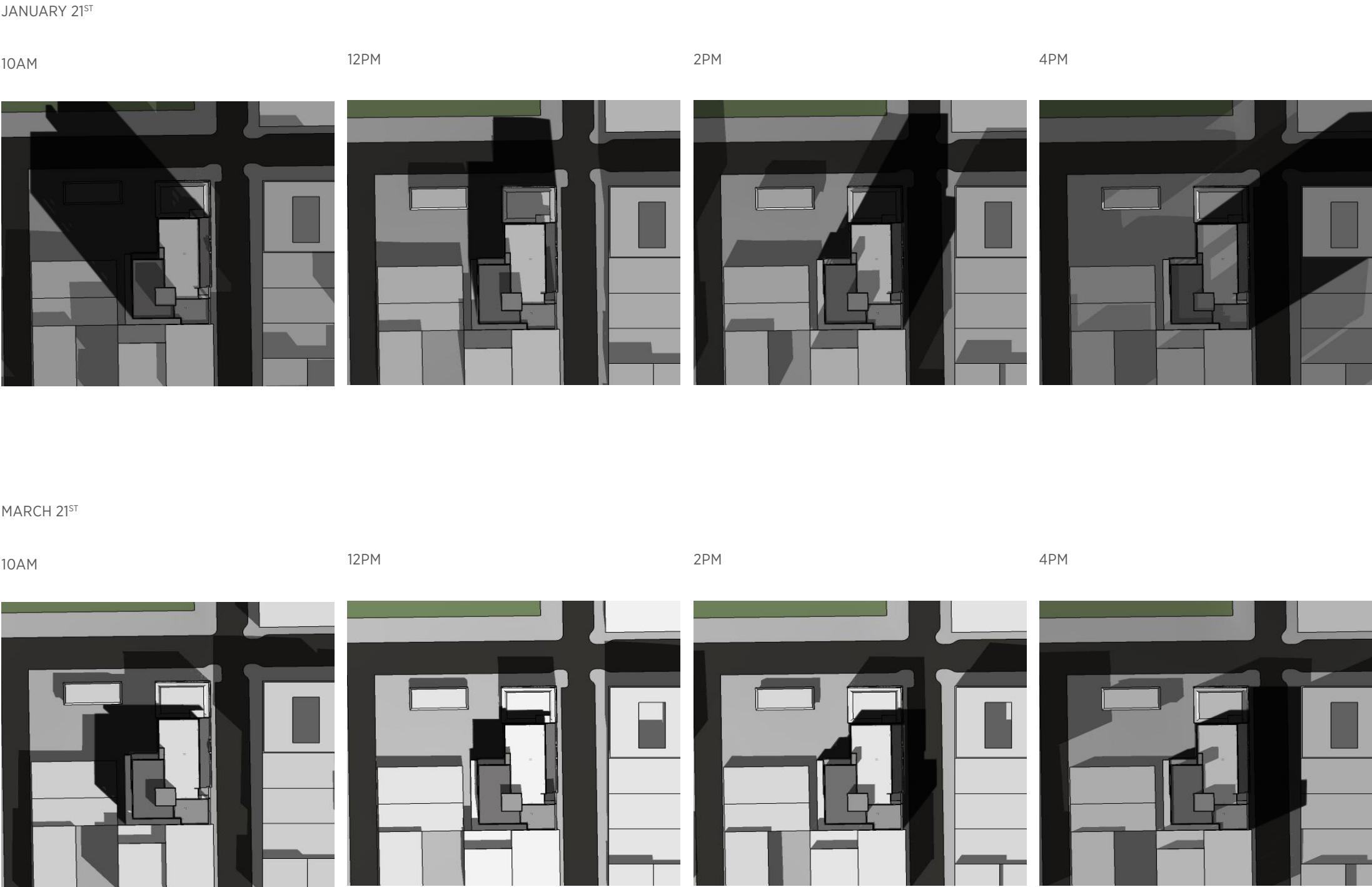
PROPOSED GARAGE ENTRY



**PAGE INTENTIONALLY  
LEFT BLANK**



SHADOW STUDY



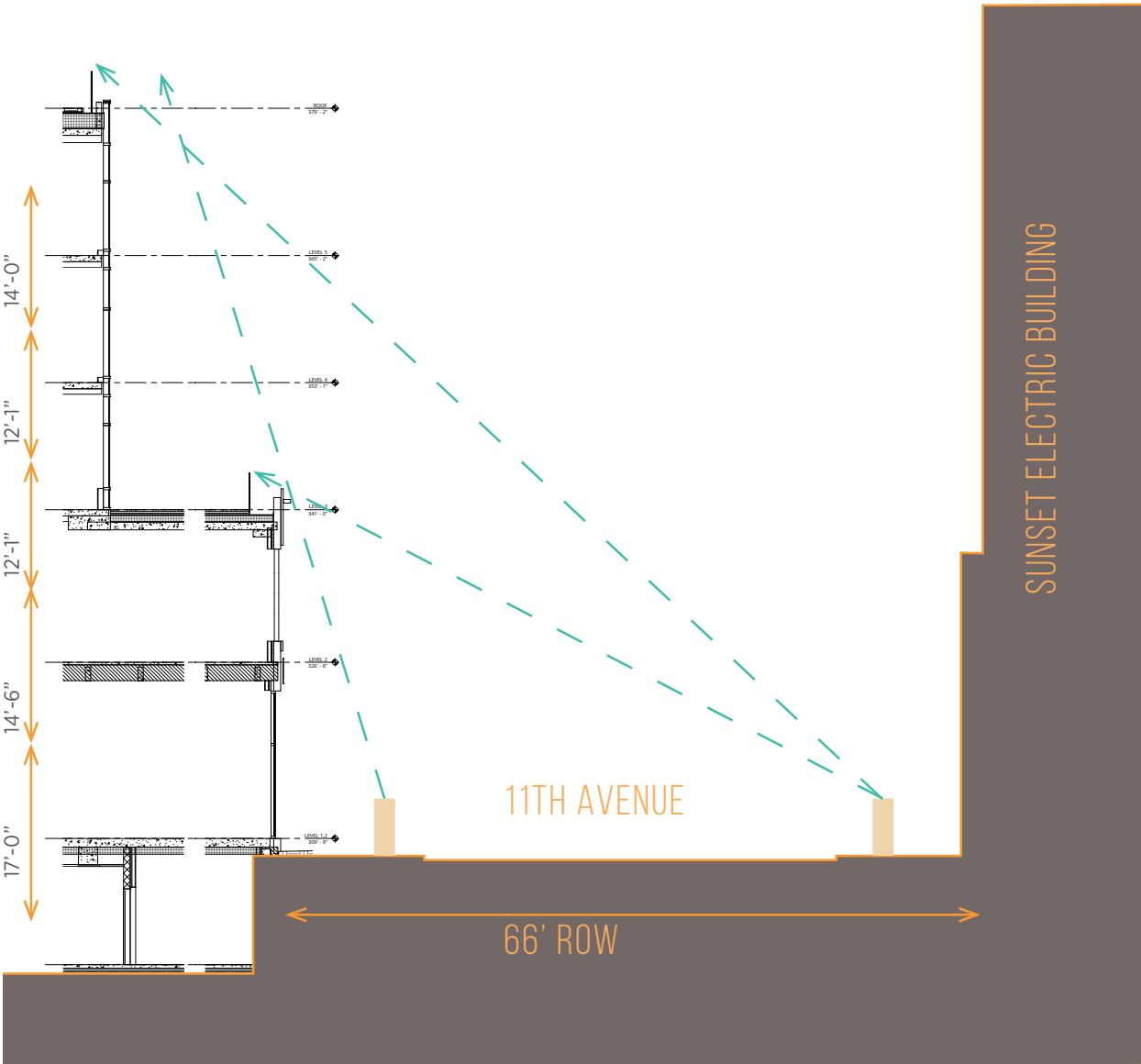


SHADOW STUDY

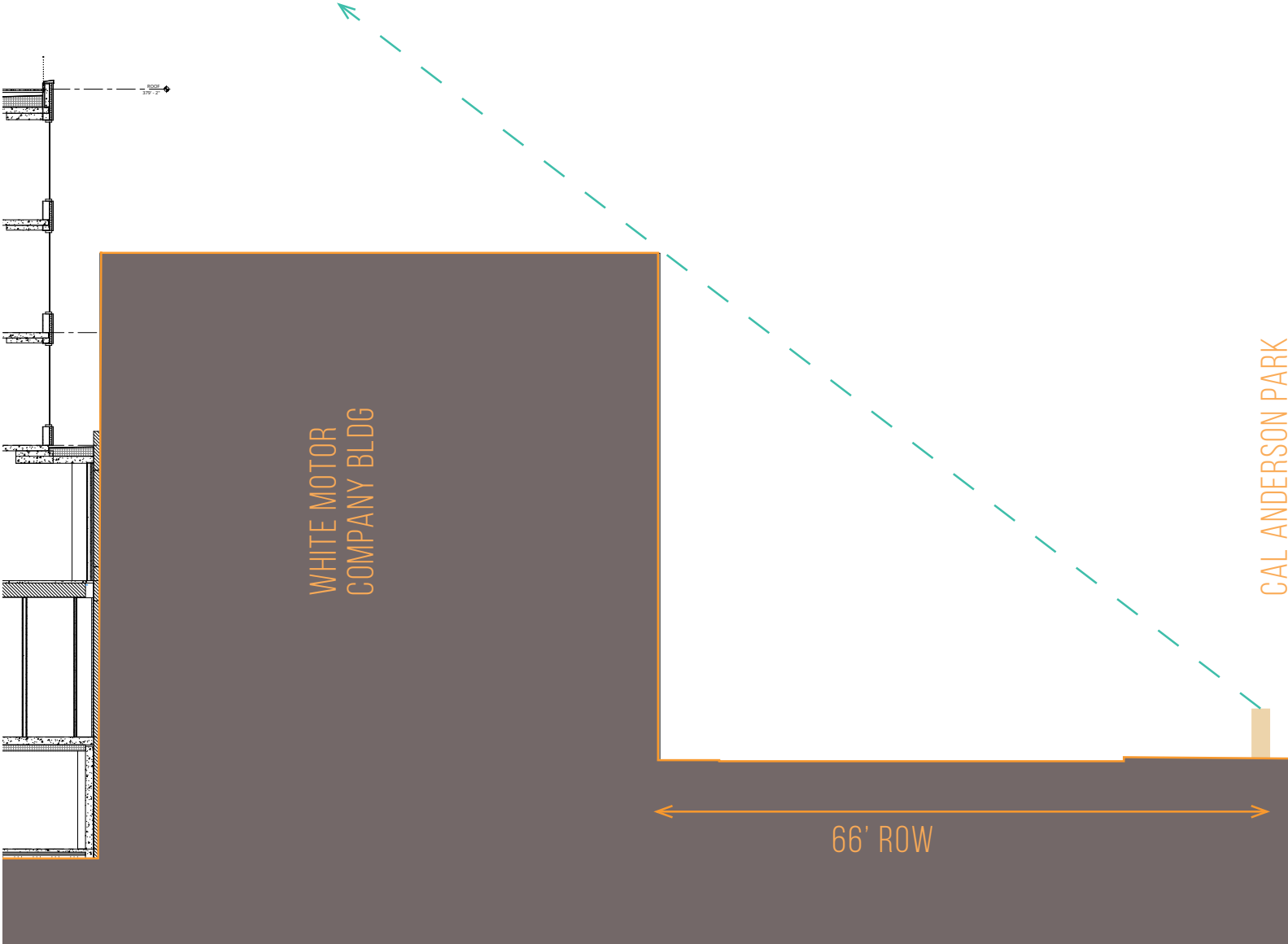




SIGHTLINE DIAGRAMS



11TH AVE SIGHTLINES DIAGRAM



PINE STREET SIGHTLINES DIAGRAM



**PAGE INTENTIONALLY  
LEFT BLANK**





THE  
ENVELOPE  
TELLS THE  
TRUTH

SUNSET ELECTRIC CO.

AUTHORIZED FACTORY SERVICE  
**DEWEY'S**  
MOTOR SERVICE Inc.  
ELECTRICAL TUNE-UP

MAGLES 2ND  
B43 L-18

1525 11 AVE.

SEATTLE OFFICE	PORTLAND OFFICE
1505 5TH AVE	6720 SW MACADAM
SUITE 300	SUITE 100
SEATTLE, WA	PORTLAND, OR
206.576.1600	503.245.7100
WWW.ANKROMMOISAN.COM	

1021 E. PINE